

Misc. - 4316 IL Route 22:
Route 22 Widening

David Lothspeich

To: David Lothspeich
Subject: RE: 4316 IL Route 22 Property Acquisition

From: David Lothspeich
Sent: Friday, May 13, 2016 1:16 PM
To: 'Harris, Michael'
Cc: Murphy, Kimberly K.; Brown, Lori S.; Czaplicki, Scott D; James Hogue; Rios, Jose
Subject: RE: 4316 IL Route 22 Property Acquisition

Michael,

Thank you for the quick reply and attachment. I want to be clear that the Village of Long Grove does conditionally support the widening and would like to be included in the conversations with the property owners so that we are informed as well. Since I expect that the property owners will continue to reach out to the Village for assistance, we would ask to be included to help both sides. In my opinion, the worst possible situation is not having sufficient information, or conflicting information, and having the residents show up at Village Board meetings, etc. asking questions and complaining of not being heard by IDOT or the Village. Ultimately this is an IDOT project and it is IDOT's responsibility to address these questions/concerns and by working together I am hopeful that the Village can provide assistance to our residents and IDOT.

Thanks,

Dave

From: Harris, Michael [<mailto:Michael.Harris2@illinois.gov>]
Sent: Friday, May 13, 2016 1:04 PM
To: David Lothspeich
Cc: Murphy, Kimberly K.; Brown, Lori S.; Czaplicki, Scott D; James Hogue; Rios, Jose
Subject: RE: 4316 IL Route 22 Property Acquisition

Mr. Lothspeich,

My Superiors and I have spoken with the Prendergast's and are attempting to explain the situation to them. I have, however, attached the information pertaining to this proposed acquisition.

Sincerely,

Michael Harris
Illinois Department of Transportation
Bureau of Land Acquisition
847-705-4285

From: David Lothspeich [<mailto:dlothspeich@longgrove.net>]
Sent: Friday, May 13, 2016 1:01 PM
To: Harris, Michael
Cc: Murphy, Kimberly K.; Brown, Lori S.; Czaplicki, Scott D; James Hogue
Subject: RE: 4316 IL Route 22 Property Acquisition

Michael,

Please provide an explanation and supporting documentation as to 4316 IL Route 22 acquisition.

While the Village of Long Grove has long gone on record supporting the widening of IL Route 22, this support has been conditioned upon minimizing the impact on the adjoining properties. In meeting with the IDOT appraiser and property owners this morning, the documents that were provided to the appraiser were inadequate to determine the extent of the impact on the properties. In an effort to assist IDOT with communicating with the Residents, I would ask that IDOT provide the following to the Village:

1. Stake the existing ROW limit, proposed ROW limit;
2. Provide a survey showing the existing ROW limit and proposed ROW limit;
3. Provide a tree survey that corresponds to the existing tree tags (the tree survey materials provide to the appraiser had tree numbers that did not correspond to the tree tags located on the trees);
4. Provide the "stock" proposed temporary easement document/language;
5. Confirm the details of all work proposed within the proposed temporary and permanent easements, include tree locations, grading, etc. to evaluate impact, and
6. Confirm whether the current plans include the proposed pathway located along the southern side of IL Route 22. If so, please provide the details to evaluate impact of pathway on proposed acquisition and grading, trees, etc.

Upon receipt of these materials, the Village will review and would then request a meeting with IDOT and the affected property owners to discuss the project and determine the impact on the properties. IDOT and the Village meet individual property owners on site may be necessary but for efficiency sake, having a single meeting to answer the questions would help to answer the majority of the questions. Any meetings with individual property owners should include the appraiser and a representative from IDOT that can answer questions relative to the times noted above.

Thank you in advance for your assistance.

Dave

David Lothspeich
Village Manager
Village of Long Grove, Illinois
847-634-9440

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From: Czaplicki, Scott D [<mailto:Scott.Czaplicki@illinois.gov>]
Sent: Friday, May 13, 2016 11:14 AM
To: David Lothspeich
Cc: Harris, Michael; Murphy, Kimberly K.; Brown, Lori S.
Subject: RE: 4316 IL Route 22 Property Acquisition

Dave,

Since the land acquisition process has begun I'll defer to Michael Harris, but please note this issue is discussed in the project report and environmental documents provided on CD to the Village with the letter of intent package in October 2014. Some references are Section 5.16 in the Combined Design Report (Volume I), Section 2.52, Section 4 and Appendix C/Exhibit C-3 of the Environmental Class of Action Determination (ECAD), and the Biological Assessment (BA).

Scott Czaplicki, P.E.
(847) 705-4107
scott.czaplicki@illinois.gov

From: David Lothspeich [<mailto:dlothspeich@longgrove.net>]
Sent: Thursday, May 12, 2016 11:34 AM
To: Czaplicki, Scott D
Cc: 'David Lothspeich'
Subject: 4316 IL Route 22 Property Acquisition

Scott,

We have a resident that lives along IL Route 22 that owns the property located at 4316 IL Route 22. It appears that this property includes a Illinois Nature Preserve on the northern portion (White Fringed Orchard) and IDOT has contacted the property owner to acquire the entire property. While I suspect that this purchase offer has something to do with the White Fringed Orchard, if you could please provide information on why IDOT is proposing to acquire the entire property I would greatly appreciate it. The resident is telling us that the appraiser informed her that the Village President approved IDOT's acquisition of the entire property and the resident is very upset with the Village and not interested in selling the entire property to IDOT (see below for a portion of our email conversations).

Thanks,

Dave

David Lothspeich
Village Manager
Village of Long Grove, Illinois
847-634-9440

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From: David Lothspeich
Sent: Thursday, May 12, 2016 11:01 AM
To: 'Darlene Demor'
Cc: James Hogue
Subject: RE: Do we need any special forms to apply for a zoning change - 4316 IL Route 22

Darlene,

The appraiser's comments that you reference are not correct and the Village has not, nor would be in a position, to approve any such arrangement for the purchase of land by the State. I will have a similar conversation with the appraiser to correct any misunderstandings.

Thanks,

Dave

From: Darlene Demor [<mailto:ddemor@yahoo.com>]
Sent: Thursday, May 12, 2016 10:03 AM
To: David Lothspeich
Subject: Re: Do we need any special forms to apply for a zoning change - 4316 IL Route 22

Hi Dave,

Thanks for your time. I will be sending the information to you later today. However, to take someone's land to plant orchids in this state that is broke, doesn't make any sense. The wetlands have three orchids, to take land to replant the orchids is not acceptable. What is less acceptable is for the mayor per the conversation with the appraiser that she approved the plans. The future value of our investment is not being considered. It is a loss of future revenue for us and for Long Grove. It is not right for the Village to approve such an arrangement and for the state to think they can steal someone's land. When Richard purchased this land from Coffin's it was not to later donate it to the state but for an investment.

In addition why the 70 feet - we do not want a bike path. No one asks us.

Darlene & Richard Prendergast

On Thursday, May 12, 2016 9:14 AM, David Lothspeich <dlothspeich@longgrove.net> wrote:

Darlene,

Thanks for the call yesterday, when you have a moment please forward the State Appraiser contact information so that I can try to find out more about why the State has interest in the entire property.

As noted in our conversation, neither the Village, Angie or any other member of the Board of Trustees "okayed this steal". IL Route 22 is a State Highway and the State is developing the plans to widen the highway from 2 to 4 lanes. The Village of Long Grove has officially gone on record in 2007 and 2014 by approving resolutions (attached) conditionally supporting the improvements with the conditions primarily focused on limiting the overall width of the project and the associated impact on the natural environment and adjoining residential properties. While I suspect that the State is proposing the acquisition of the property to ensure the protection of the endangered White Fringed Orchard and expansion of the existing IL State Nature Preserve, I do not know that for sure and plan to have further conversations with IDOT to confirm.

Thanks,

Dave

From: Darlene Demor [<mailto:ddemor@yahoo.com>]

Sent: Monday, May 09, 2016 1:46 PM

To: David Lothspeich

Subject: Re: Do we need any special forms to apply for a zoning change - 4316 IL Route 22

Hi Dave,

We need to talk because the state wants to take our property at 4316 that is contiguous with 4339 - the property we live at. We have paid taxes on that property and have other uses for it than donate it to the state. The orchids of which we have never seen any, are in the wetlands which can be used for activity in the right developer was to buy both parcels. The town is losing revenue and the state in essence is getting for free from our hard earned dollars. Do you see what the state pays for what it has taken for forest preserve. The area where the house sits is what the state wants not the wetlands. In addition they want an acre from our house going back 70 feet from the road.

It is our understanding Angie okayed this steal - Is she giving up her land? This is highway robbery. This is what people complain about - again what about the future value this land could bring if bought as a whole by a developer.

We would like to talk about this. We would rezone both parcels that is the only way to recoup the loss we would be hit with by the state taking 10 acres from us.

Darlene & Richard Prendergast

On Monday, May 9, 2016 12:55 PM, David Lothspeich <dlothspeich@longgrove.net> wrote:

Darlene and Richard,

As a quick follow-up to your discussion and inquiry with Trustee Michael Sarlitto, I offer the following information:

The Village is aware of the project and has gone on record formally supporting the widening of IL Route 22 with specific limitations to reduce the impact of the widening on the adjoining Long Grove properties. While the majority of the improvements remain located within the existing ROW, there are instances where IDOT has contacted property owners about temporary easements and permanent acquisition. The temporary easements are typically for grading or other improvements that are necessary to accommodate the wider road that do not require ownership by the State. With the exception of the proposed acquisition of your property by the State, the permanent acquisition is very limited to accommodate improvements such as the traffic signal equipment for IL 22 and N. Krueger (Please refer to the following links of Historical documents re: the Village of Long Grove and IDOT IL Route 22 Widening Plans re: IDOT plans presented at their public meeting and public hearing.

Historical:

<http://www.longgrove.net/sites/default/files/06i%20-%20IL%20Route%2022%20Widening%20-%20Long%20Grove%20%26%20IDOT%20Historical.pdf>

IDOT IL Route 22 Widening Plans:

<http://www.longgrove.net/sites/default/files/06ii%20-%20IDOT%20Plans%20For%20Widening%20Of%20IL%20Route%2022.pdf>

My limited understanding of your property located at 4316 IL Route 22 is that it contains a Illinois Nature Preserve on the northern portion of the property. I believe that this designation was placed on the property years ago to protect the White Fringed Orchard (Threatened Species on the U.S. List of Endangered and Threatened Wildlife and Plants on September 28, 1989.) that is on the property. The State's plans for IL Route 22 include the proposed Water Quality Basin situated on the north side of IL Route 22 directly across from your property to protect the water quality of the Nature Preserve. Further information on the White Fringed Orchard is available through the following link: <http://www.fws.gov/midwest/Endangered/plants/prairief.html>.

In terms of potential zoning change/development, the property located at 4316 IL Route 22 includes the following restrictions/limitations:

The property located at 4316 IL Route 22 is shown as being approximately 8.93 acres and is currently zoned R-1 single-family residential, minimum 3-acre lots size. The property may be encumbered with a restricted Nature Preserve that appears to be roughly 2-3 acres, plus/minus. In addition, the property includes various drainage limitations; including: floodplain (FEMA); ADID Wetland and Lake County Wetland shown on the attached maps (these areas would need to be confirmed by a licensed surveyor/engineer):

- 1 - Drainage FEMA (2016)
- 2 - ADID With 100 Ft. Buffer
- 3 - Lake County Wetland

From a Village zoning/development perspective, the subdivision or development of the property would be limited by these restrictions. The Village's Comprehensive Plan does not contemplate your property, or any other property situated off of IL Route 22 other than at Old McHenry Road, as being zoned/developed as anything other than what currently exists. That said, as with any property owner, you have the right to make application to re-zone/develop the property. If you would like further information, please contact Village Planner James Hogue (copied) for further information. I have been in contact with one of the appraisers that has been hired by IDOT but based upon the information they provided, it does not appear that he is the appraiser assigned to your property. If you would please share the IDOT appraiser information, I can contact him/her to request a copy of the letter and other materials specific to your property and would be more than

willing to meet with you and the appraiser to further discuss their plans if that is something that you would be interested in.

Please let me know if I can be of further assistance.

Thanks,

Dave

David Lothspeich
Village Manager
Village of Long Grove, Illinois
847-634-9440

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From: Michael Sarlitto [<mailto:sarlitto.lgvillage@gmail.com>]
Sent: Thursday, May 05, 2016 5:23 PM
To: 'Darlene Demor'
Cc: Michael Sarlitto; David Lothspeich
Subject: RE: Do we need any special forms to apply for a zoning change

Darlene and Richard – it was a genuine pleasure spending time with you discussing the many activities of the village and our community and thank you for forwarding the information pertaining to the proposed Rte. 22 expansion project including details that indicate potentially adverse consequences for you and surrounding neighbors.

I've reached out to Village Manager David Lothspeich to set up a meeting to discuss the details/impacts of the project from the Village's perspective and am hopeful we should be able to meet in the very near future.

As always, feel free to forward on any other developments as you learn of them and I will try my best to come up to speed as quickly as possible on the details of the project/situation.

Thanks again for your time and I am sure we'll be discussing the topic again very soon,

Mike

Michael Sarlitto
Trustee
Village of Long Grove

847-204-1922 (mobile)
Skype: michael_sarlitto

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From: Darlene Demor [<mailto:ddemor@yahoo.com>]
Sent: Thursday, May 05, 2016 2:43 PM
To: Michael Sarlitto
Subject: RE: Do we need any special forms to apply for a zoning change

Mike:

As you can see from the PDF map I sent our house is the only one in Long Grove they want to take for not the road but a forest preserve. The other five properties are in Kildeer. Apparently the Long Grove board approved our acquisition for a forest preserve. We finally talked with IDOT acquisition,

Michael Harris, after of which we left four messages but he said he did not get any messages. We asked how could they take private property for a forest preserve and isn't this illegal? He would not answer the questions and directed me to John Fortman - Regional Engineer or Bruce Rauner - so I called Bruce Rauner's office and left a message and number with his assistant. I really doubt we will get a return call.

So our next step is to talk with our neighbors this week and see what their feeling are on a zone change. Taking the 10 acres from the 32 and leaving us with 22 will diminish the value of the land. Have to make the difference in value up somehow so changing the zoning is the only way we can think of making the difference and then moving. Why would we want to stay on a piece of property that is now diminished in value but the taxes just keep going up. We pay over \$50,000 in property taxes for the state to walk in a take away what we have worked for. And supposedly Angie approved this. We are not a charity. Just research what the state pays out in acquisition prices - it is pennies on a dollar.

So do we need any special forms when we talk with our neighbors and hopefully get them to sign off that they would not mind the property being used for a useful purpose.

Thank You

Darlene & Richard Prendergast

From: Darlene Demor [<mailto:ddemor@yahoo.com>]

Sent: Tuesday, May 03, 2016 12:12 PM

To: Michael Sarlitto

Subject: RE: Promised PDF for Route 22

Hi Mike:

Thanks for meeting yesterday. Per our conversation attached please find the website for Route 22 construction. They plan on taking 6 residences. The 9 acres they want from us is for a nature forest preserve and has nothing to do with the road. They just want to tie it in.

I think we will approach our neighbors to see if they based on the new 22 road construction would agree to a proposal for our property to be reclassified as commercial.

Looks like from the date the goal of IDOT is regional linkage. The appraiser called this morning and said the village knows about the the construction. Taking viable productive property that can be income producing for the homeowner and possible town and turning it into a land grab for a forest preserve is not right.

Let me know if there are any particular forms or format that we need when talking to our surrounding neighbors. There are not many.

Thanks again for the meeting

Darlene & Richard

<http://www.idot.illinois.gov/Assets/uploads/IDOT-Projects/District-1/IL-Rt-22/Preferred%20Alternative.pdf>

<http://www.idot.illinois.gov/projects/Illinois-Route-22-Study>

<http://www.idot.illinois.gov/Assets/uploads/IDOT-Projects/District-1/IL-Rt-22/Resource%20Impacts.pdf>

From: David Lothspeich

Sent: Thursday, August 15, 2013 4:19 PM

To: Czaplicki, Scott D

Subject: RE: IL Route 22 Widening Public Hearing Notice - July 16, 2013

Scott,

Thank you very much for your detailed response and for keep Long Grove's concerns in mind as you move forward with this project.

Thanks,

Dave

From: Czaplicki, Scott D [<mailto:Scott.Czaplicki@illinois.gov>]

Sent: Thursday, August 15, 2013 1:02 PM

To: David Lothspeich

Subject: RE: IL Route 22 Widening Public Hearing Notice - July 16, 2013

Dave,

Coordination on the retaining wall aesthetics will be coordinated during the contract plan preparation phase (Phase II). We have a Special Design/Construction Consideration (SDCC) in the Combined Design Report to ensure this happens. We also have SDCCs to coordinate with the Village for downtown wayfinding signage, replacing trees near where they were removed, coordinating with the Village regarding the landscaping design including landscaping the median along the south leg of Old McHenry Road, and providing pedestrian crossings/signals along the west and south approaches of the Old McHenry Road intersection.

Regarding the other comments, the posted speed will be 45 mph, which is the maximum allowable with curb and gutter along the roadway edges. The roadway improvements are limited to the Ex ROW, but the path grading extends into SCEs. We did receive a request from Lake County Division of Transportation to reinstate the right turn lane along Old McHenry Road into Long Grove Common's southern entrance since the developer was required to build it. If we reinstate it, there may be some encroachment into the SCE for grading. We will design and let you know the extent of the encroachment. We did receive one comment from the hearing to add the temporary signal at North Krueger Road (I thought there would be more), but additional through lanes along IL 22 are required to implement this. The Phase II consultant will begin the design of this traffic signal, as well as the entire project, this Fall once we complete the study.

Thank you for the comments.

Scott Czaplicki, PE

201 W. Center Court
Schaumburg, IL 60196-1096
(847) 705-4084 Office
(630) 291-0869 Mobile
scott.czaplicki@illinois.gov

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From: David Lothspeich [<mailto:dlothspeich@longgrove.net>]

Sent: Thursday, August 15, 2013 11:30 AM

To: Czaplicki, Scott D

Subject: RE: IL Route 22 Widening Public Hearing Notice - July 16, 2013

Scott,

Thank you for the follow-up. I haven't received any other comments from the residents but the Village Board did review and provided the following comments:

- maintain 45 mph speed limit.
- limit improvements within existing ROW
- install temporary signal at IL 22 and N. Krueger (Village has requested a temporary signal for more than 10 years and none installed since the improvements have been anticipated to be completed in 5 years)

In general, the Village Board is interested in utilizing other materials and design to improve the aesthetics of the retaining walls but would need to see alternatives and have cost estimates.

Thanks again,

Dave

From: Czaplicki, Scott D [<mailto:Scott.Czaplicki@illinois.gov>]
Sent: Monday, August 12, 2013 5:15 PM
To: David Lothspeich
Subject: RE: IL Route 22 Widening Public Hearing Notice - July 16, 2013

Dave,

Sorry I missed you at the hearing. Did you receive any comments regarding the stakeout of the shared-use path? We received one comment requesting the walls along the path at Stonehaven be decorative. Would the Village be willing to cost participate for this? We are starting to address the comments from the hearing. Please let me know if the Village would like any revisions made to the path. If you have any questions, please call. Thank you.

Scott Czaplicki, PE

201 W. Center Court
Schaumburg, IL 60196-1096
(847) 705-4084 Office
(630) 291-0869 Mobile
scott.czaplicki@illinois.gov

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From: Czaplicki, Scott D
Sent: Monday, July 08, 2013 6:25 PM
To: 'David Lothspeich'
Cc: Baczek, John A; Murphy, Kimberly K.; dwbblock@transystems.com
Subject: RE: IL Route 22 Widening Public Hearing Notice - July 16, 2013

Dave,

The stakeout is scheduled for Monday (July 15) and Tuesday, if needed. We will stakeout the existing right-of-way and either the edge of the path or retaining wall, and provide stationing on the stakes so you can cross reference with the plans and cross sections. PDFs of the plans and cross sections have been forwarded though a separate e-mail. Please let me know if you have any questions.

Scott Czaplicki, PE

201 W. Center Court
Schaumburg, IL 60196-1096
(847) 705-4084 Office
(630) 291-0869 Mobile
scott.czaplicki@illinois.gov

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From: David Lothspeich [<mailto:lothsd@longgrove.net>]
Sent: Monday, July 08, 2013 2:57 PM

To: David Lothspeich; James R. Woods, P.E., PTOE; Baczek, John A; Czaplicki, Scott D
Cc: Schneider, Paul A; Joseph J. Emry, P.E.; cjstenzel@transystems.com; Brian Witkowski; Marc Small
Subject: RE: IL Route 22 Widening Public Hearing Notice - July 16, 2013

All,

The Village received the attached notice and plans today (July 8) for the upcoming July 16 public hearing. FYI, my original email (below) to Paul Schneider and CJ Stenzel were returned as undeliverable.

No need to send plans since they were attached to the notice but if you could please confirm when/if IDOT can stake the ROW I'd greatly appreciate it so that we can review with the affected property owners.

Thanks,

Dave

From: David Lothspeich
Sent: Monday, July 08, 2013 2:30 PM
To: 'James R. Woods, P.E., PTOE'; 'John.Baczek@illinois.gov'; 'Scott.Czaplicki@illinois.gov'
Cc: 'Schneider, Paul A'; 'Joseph J. Emry, P.E.'; 'cjstenzel@transystems.com'; 'Brian Witkowski'; David Lothspeich; Marc Small
Subject: IL Route 22 Widening Public Hearing Notice - July 16, 2013

All,

The Village President received the attached letter (dated June 25, 2103) from IDOT announcing the public hearing scheduled for July 16, 2103. The Village President owns property that has access to IL Route 22 and also received a postcard announcing the public hearing. Until I met with the Village President today, this is the first that I've heard of the public hearing and the referenced "preferred alternative". The plans are not yet posted on the website noted in the letter www.ilroute22.org so the Village is not aware the specifics of the "preferred alternative" and cannot offer any further direction that what was offered in 2010 (see attached).

In reviewing my files my most recent communications that I could find re: IL Route 22 widening were from 2011. My recollection is that the Village was requesting that the limits of the planned IL Route 22 improvements (with pathway and without pathway) be staked along IL Route 22 between North Krueger and IL Route 83 so that we could evaluate the impact of including a pathway on the adjacent property owners. To the best of my knowledge, this requested staking was never done and the Village therefore did not have an opportunity to review with the affected property owners.

The Village of Long Grove has gone on record since 2007 in support of the widening of IL Route 22. While I suspect that the Village Board may still support the widening of IL Route 22, the Village has a new Board of Trustees since 2007 and they have not had an opportunity to review the plans and to develop a position and receiving notice approximately two week prior to the public hearing doesn't afford our Board the opportunity to consider this matter until after the public hearing. I expect that the Village will receive calls from residents that received the notice and unfortunately we cannot provide much of a response since the most recent information from IDOT is from two years ago (2011) and still do not fully understand the impact of the planned improvements on the adjacent properties.

The Village requests copies of the "preferred alternative" plans that will be presented during the July 16, 2013 public hearing and once again requests that the improvement limits be staked so that we can review with the affected residents.

Thanks,

Dave

From: David Lothspeich
Sent: Tuesday, October 07, 2008 2:21 PM
To: 'James R. Woods, P.E., PTOE'; 'Brian Witkowski'; 'mreznicek@esiconsultantsltd.com'
Cc: 'Schneider, Paul A'; Joseph J. Emry, P.E.; 'John.Baczek@illinois.gov'; 'cjstenzel@transystems.com'
Subject: RE: 1210 - IL Route 22 Coordination Meeting with Long Grove - Minutes

Jim,

Looks good to me. One suggestion re: Route 22 & Old McHenry and Route 22 & N. Krueger, the Village is requesting the crosswalks WITH pedestrian activated signals.

Thanks,
Dave

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From: James R. Woods, P.E., PTOE [<mailto:JWoods@civiltechinc.com>]
Sent: Friday, October 03, 2008 5:35 PM
To: 'Brian Witkowski'; 'mreznicek@esiconsultantsltd.com'; David Lothspeich
Cc: 'Schneider, Paul A'; Joseph J. Emry, P.E.; 'John.Baczek@illinois.gov'; 'cjstenzel@transystems.com'
Subject: 1210 - IL Route 22 Coordination Meeting with Long Grove - Minutes

All,

Attached please find meeting minutes from the 9/30/2008 coordination meeting at IDOT.

Please review and respond with comments within one week (by end of day 10/10/2008).

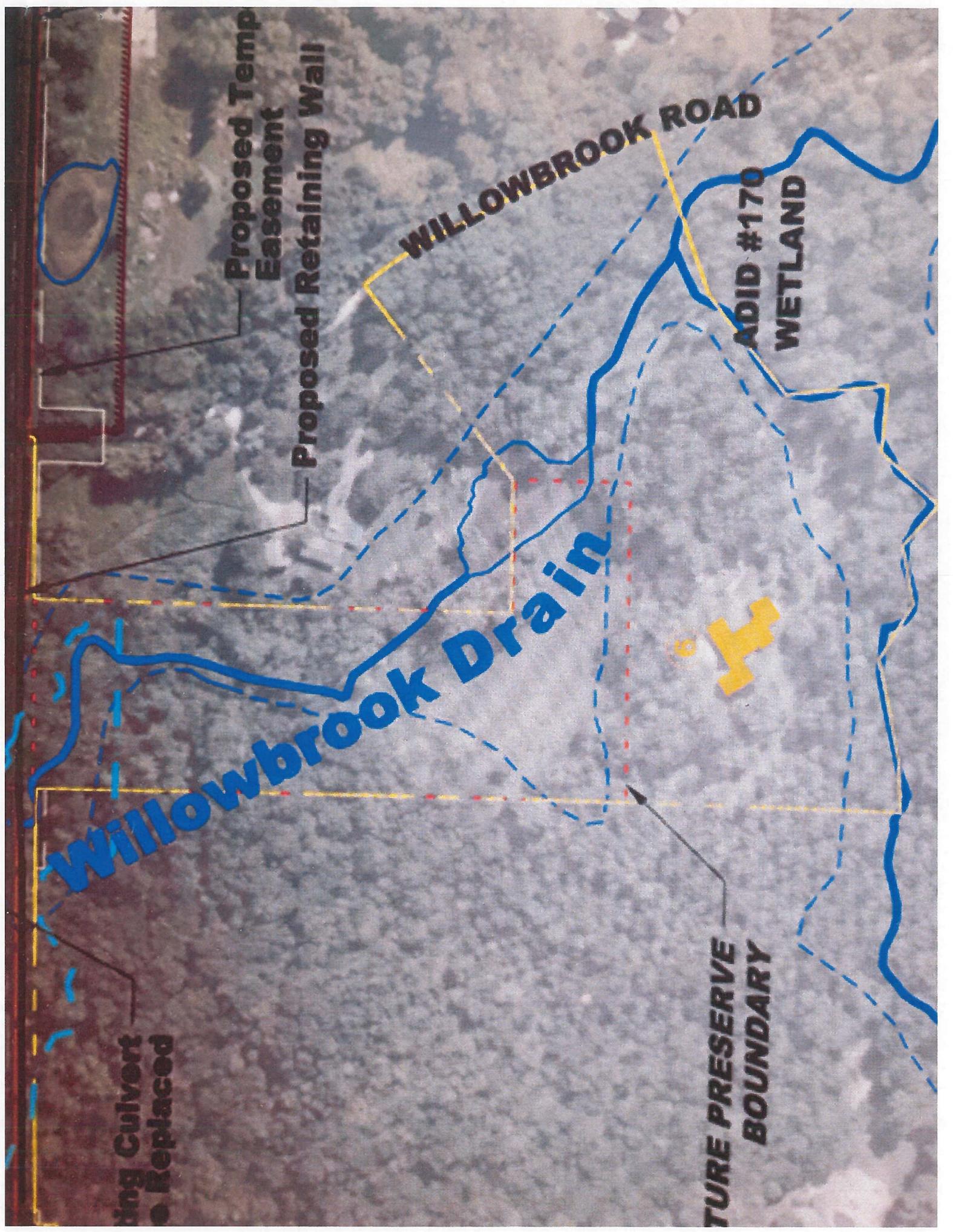
If no comments are received, these will become final and part of the project record.

Thank you,
Jim



James R. Woods, P.E., PTOE
Project Manager
CIVILTECH ENGINEERING, INC.
450 E. Devon Ave, Suite 300
Itasca, IL 60143

Direct: 630.735.3942
Phone: 630.773.3900
Fax: 630.773.3975
www.civiltechinc.com



WILLOWBROOK ROAD

**ADID #170
WETLAND**

**Proposed Temp
Easement**

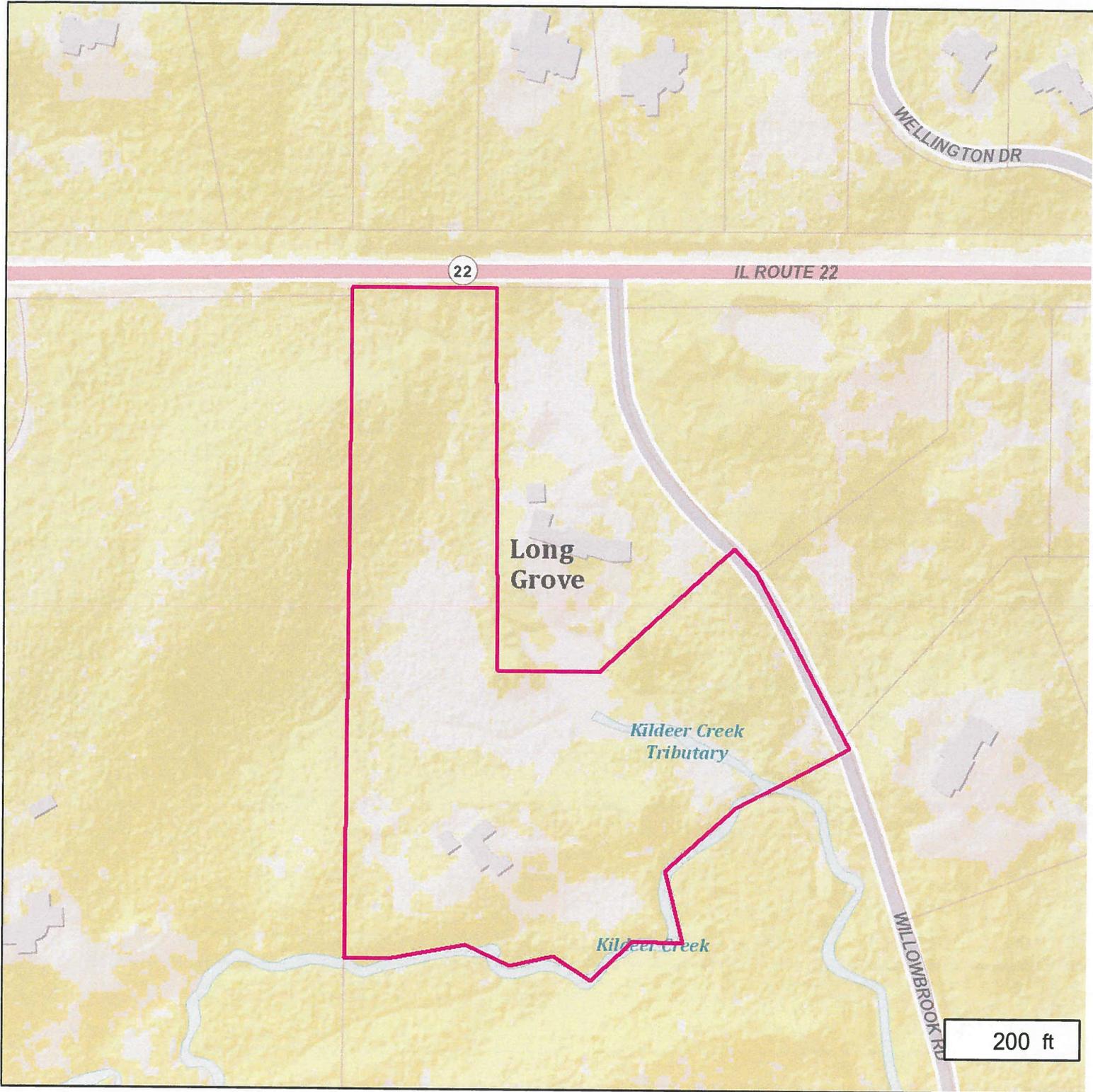
Proposed Retaining Wall

Willowbrook Drain

**ATURE PRESERVE
BOUNDARY**

**ing Culvert
e Replaced**

Lake County, Illinois



 **LakeCounty**
Geographic Information System

Lake County
Department of Information Technology
18 N County St
Waukegan IL 60085
(847) 377-2373

Map Printed on 5/9/2016
Parcel 1519100001 is outlined.

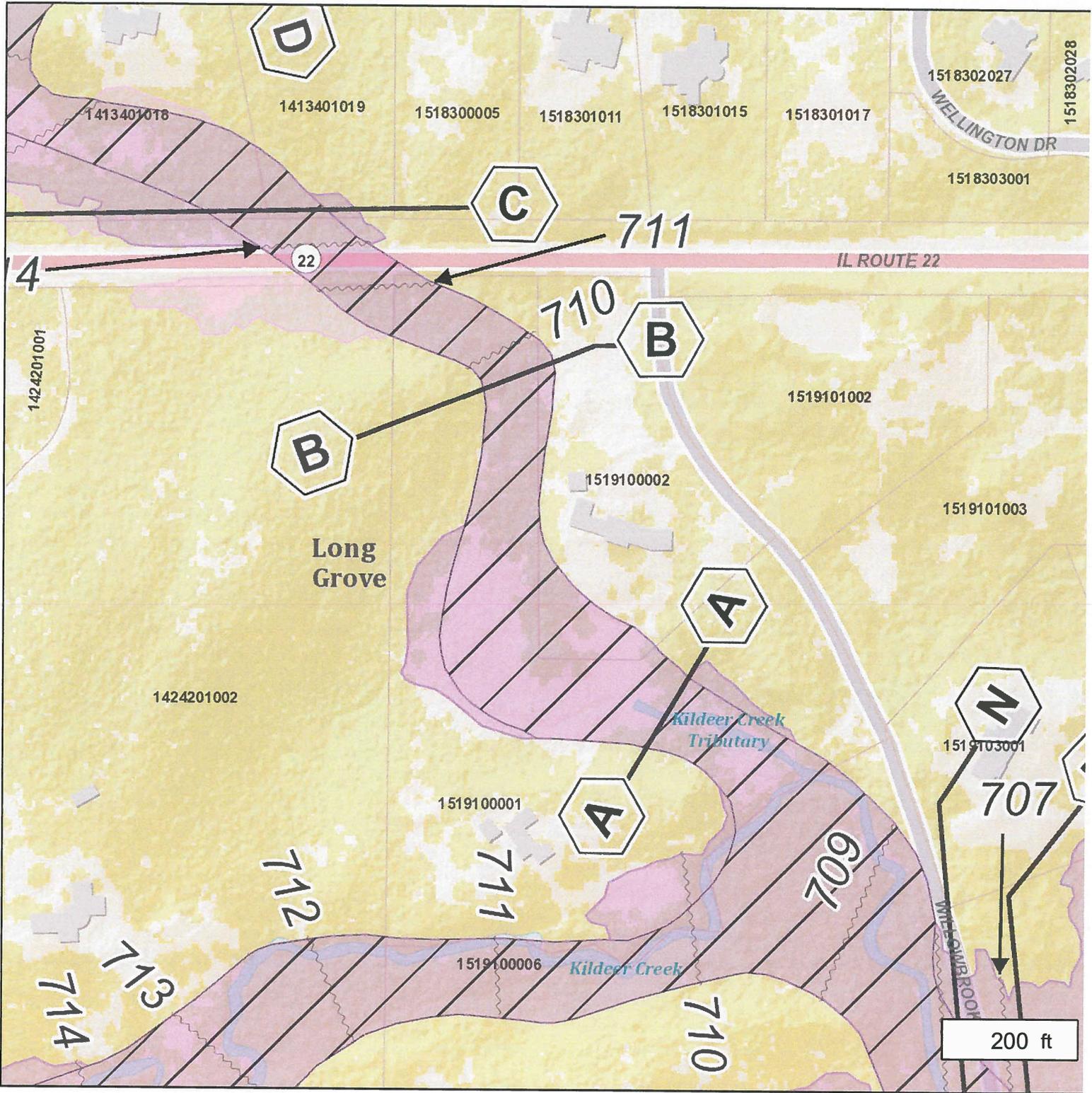


-  Tax Parcels
-  Municipalities

Disclaimer:

The selected feature may not occur anywhere in the current map extent. A Registered Land Surveyor should be consulted to determine the precise location of property boundaries on the ground. This map does not constitute a regulatory determination and is not a base for engineering design. This map is intended to be viewed and printed in color.

Lake County, Illinois



 **LakeCounty**
Geographic Information System

Lake County
Department of Information Technology
18 N County St
Waukegan IL 60085
(847) 377-2373

Map Printed on 5/9/2016

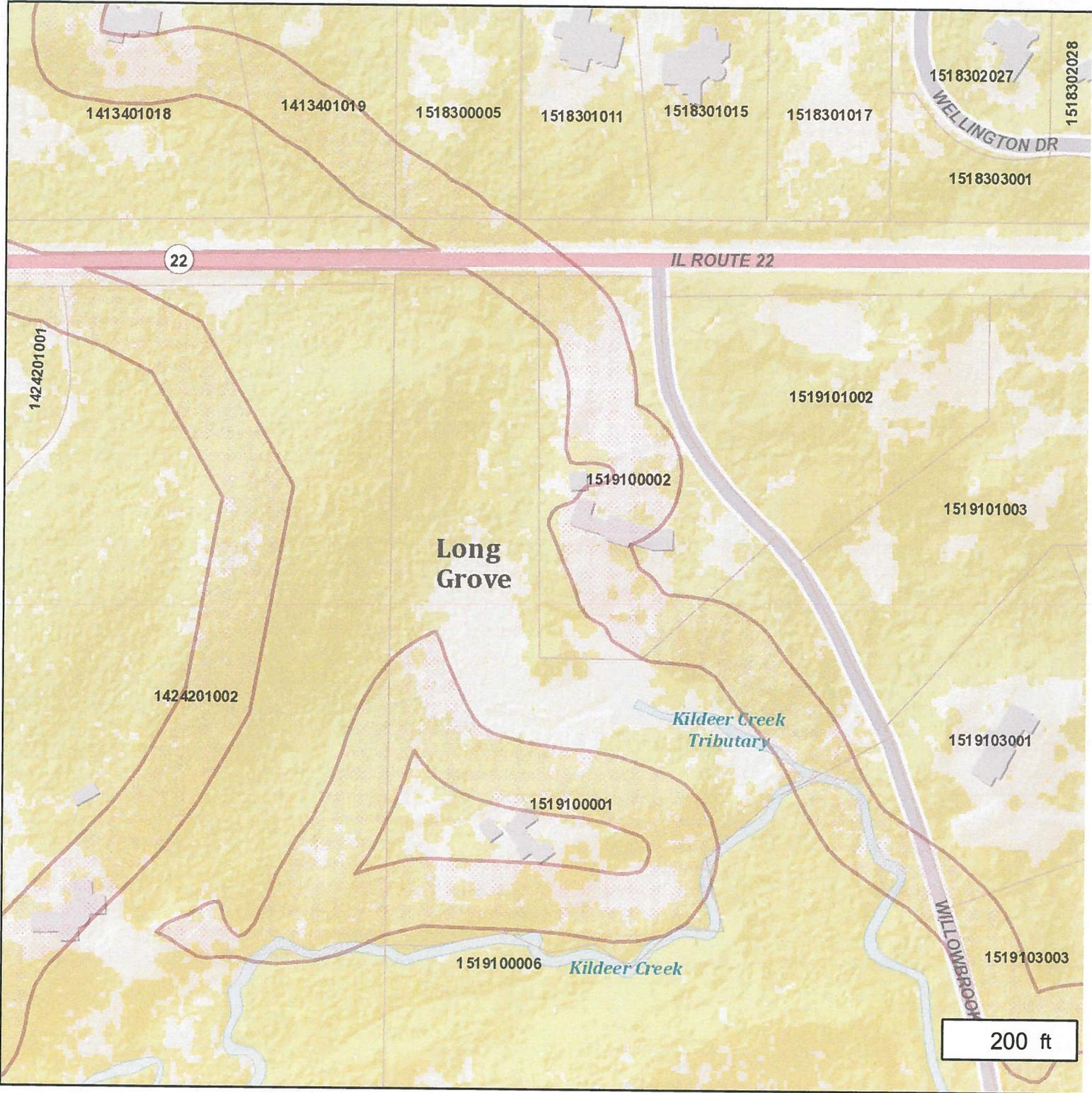


-  Tax Parcels
 -  Tax Parcels
 -  Municipalities
- ① - Drainage FEMA (2014)*

Disclaimer:

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Lake County, Illinois



 **Lake County**
Geographic Information System

Lake County
Department of Information Technology
18 N County St
Waukegan IL 60085
(847) 377-2373

Map Printed on 5/9/2016



-  Tax Parcels
-  Municipalities

② - ADD W/ 100 FT. BUFFER

Disclaimer:

The selected feature may not occur anywhere in the current map extent. A Registered Land Surveyor should be consulted to determine the precise location of property boundaries on the ground. This map does not constitute a regulatory determination and is not a base for engineering design. This map is intended to be viewed and printed in color.

Lake County, Illinois



 **LakeCounty**
Geographic Information System

Lake County
Department of Information Technology
18 N County St
Waukegan IL 60085
(847) 377-2373

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-  Tax Parcels
-  Municipalities
-  Wetlands

3 - Lake County Wetlands

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David Lothspeich

From: Baczek, John A [John.Baczek@illinois.gov]
Sent: Monday, July 08, 2013 4:23 PM
To: David Lothspeich
Subject: RE: IL Route 22 Widening Public Hearing Notice - July 16, 2013

Sorry about that Dave. You are right, we should have reached out to you before announcing we were heading to a hearing. I think we have been so mired in the environmental aspects of the project that we may have lost sight of the community concerns and needs. We will make sure we address your questions asap.

From: David Lothspeich [mailto:lothsd@longgrove.net]
Sent: Monday, July 08, 2013 2:30 PM
To: James R. Woods, P.E., PTOE; Baczek, John A; Czaplicki, Scott D
Cc: Schneider, Paul A; Joseph J. Emry, P.E.; cjstenzel@transystems.com; Brian Witkowski; David Lothspeich; Marc Small
Subject: IL Route 22 Widening Public Hearing Notice - July 16, 2013

All,

The Village President received the attached letter (dated June 25, 2103) from IDOT announcing the public hearing scheduled for July 16, 2103. The Village President owns property that has access to IL Route 22 and also received a postcard announcing the public hearing. Until I met with the Village President today, this is the first that I've heard of the public hearing and the referenced "preferred alternative". The plans are not yet posted on the website noted in the letter www.ilroute22.org so the Village is not aware the specifics of the "preferred alternative" and cannot offer any further direction that what was offered in 2010 (see attached).

In reviewing my files my most recent communications that I could find re: IL Route 22 widening were from 2011. My recollection is that the Village was requesting that the limits of the planned IL Route 22 improvements (with pathway and without pathway) be staked along IL Route 22 between North Krueger and IL Route 83 so that we could evaluate the impact of including a pathway on the adjacent property owners. To the best of my knowledge, this requested staking was never done and the Village therefore did not have an opportunity to review with the affected property owners.

The Village of Long Grove has gone on record since 2007 in support of the widening of IL Route 22. While I suspect that the Village Board may still support the widening of IL Route 22, the Village has a new Board of Trustees since 2007 and they have not had an opportunity to review the plans and to develop a position and receiving notice approximately two week prior to the public hearing doesn't afford our Board the opportunity to consider this matter until after the public hearing. I expect that the Village will receive calls from residents that received the notice and unfortunately we cannot provide much of a response since the most recent information from IDOT is from two years ago (2011) and still do not fully understand the impact of the planned improvements on the adjacent properties.

The Village requests copies of the "preferred alternative" plans that will be presented during the July 16, 2013 public hearing and once again requests that the improvement limits be staked so that we can review with the affected residents.

Thanks,

Dave

From: David Lothspeich
Sent: Tuesday, October 07, 2008 2:21 PM
To: 'James R. Woods, P.E., PTOE'; 'Brian Witkowski'; 'mrezniczek@esiconsultantsltd.com'

Cc: 'Schneider, Paul A'; Joseph J. Emry, P.E.; 'John.Baczek@illinois.gov'; 'cjstenzel@transystems.com'
Subject: RE: 1210 - IL Route 22 Coordination Meeting with Long Grove - Minutes

Jim,

Looks good to me. One suggestion re: Route 22 & Old McHenry and Route 22 & N. Krueger, the Village is requesting the crosswalks WITH pedestrian activated signals.

Thanks,
Dave

This email is intended only for the use of the individual or entity to which it is addressed, and may contain information that is privileged, confidential and exempt from disclosure under applicable law. If you are not the intended recipient, dissemination, distribution or copying of this communication is strictly prohibited. If you have received this communication in error, please notify us immediately by collect telephone call and return the original copy to us at: 3110 RFD, Long Grove, IL 60047 by US mail. We will reimburse you for postage.

From: James R. Woods, P.E., PTOE [<mailto:JWoods@civiltechinc.com>]
Sent: Friday, October 03, 2008 5:35 PM
To: 'Brian Witkowski'; 'mreznicek@esiconsultantsltd.com'; David Lothspeich
Cc: 'Schneider, Paul A'; Joseph J. Emry, P.E.; 'John.Baczek@illinois.gov'; 'cjstenzel@transystems.com'
Subject: 1210 - IL Route 22 Coordination Meeting with Long Grove - Minutes

All,

Attached please find meeting minutes from the 9/30/2008 coordination meeting at IDOT.

Please review and respond with comments within one week (by end of day 10/10/2008).

If no comments are received, these will become final and part of the project record.

Thank you,
Jim



James R. Woods, P.E., PTOE
Project Manager
CIVILTECH ENGINEERING, INC.
450 E. Devon Ave, Suite 300
Itasca, IL 60143

Direct: 630.735.3942
Phone: 630.773.3900
Fax: 630.773.3975
www.civiltechinc.com

Pathways Committee Recommendation

Illinois Route 22 Pathway

The Long Grove Pathway Committee recommends that a 10' asphalt "spine" pathway is the most appropriate type of pathway at this location on the south side of Illinois Route 22 subject to resolution of the following concerns;

Response: Thank you for providing your recommendation regarding pedestrian and bicycle accommodations for the Illinois Route 22 (IL 22) project. The path would be asphalt surface, ten-foot wide, and offset five feet from IL 22. The proposed retaining walls between Old McHenry Road and Indian Creek Lane would be removed from the plan and grading would occur in the Scenic Corridor Easements (SCE) adjacent to the path as described in IDOT's pedestrian and bicycle evaluation that was transmitted to the Village on November 11, 2010.

- 1) Why does the pathway stop at Heritage Lane?

Response: The path is shown ending at Heritage Lane because this is the IL 22 project limit where the proposed improvements match the existing roadway section. Heritage Lane is a logical terminus for the path because it provides direct connectivity to neighborhoods that have intersections with IL 22 in the study area, Quentin Road to IL 83, and minimizes right-of-way and SCE impacts. There are no existing pedestrian or bicycle facilities to connect to at the IL 22 and IL 83 intersection.

- 2) Why are so many trees being removed along the north side of Route 22?

Response: Tree removals have been minimized to the extent possible. The proposed alignment of IL 22 generally follows the existing alignment within the Village of Long Grove. As a result, pavement widening is symmetric and any unbalanced removal of trees is unintended with the exception of the area between North Krueger Road and Willowbrook Road. Compensatory floodplain storage grading and a water quality basin are proposed in this area which results in additional tree impacts.

- 3) Is a landscaping plan proposed for tree replacement and if so when will tree planting occur?

Response: A landscaping plan, including proposed tree locations, will be developed during the contract plan preparation and land acquisition phase (Phase II) of the project. IDOT has agreed to the previous Village requests to maximize the number of tree replacements within the Village, to maximize the use of Village protected species, and to include the Village in the landscape design process. Trees are generally planted once roadway construction is completed, during the tree's recommended planting season.

- 4) What is the schedule for completion of the Route 22 improvements; including the pathway?

Response: The project is currently in the preliminary engineering and environmental study phase (Phase I). Phase II and construction (Phase III) are included in IDOT's Fiscal Year 2011-2016 Proposed Highway Improvement Program, subject to funding availability and project readiness. Phase I is anticipated to be completed this year. Phase II typically takes 18 to 24 months to complete. The schedule for construction has not been determined, but can be expected to last two years, depending when construction begins. The path would be constructed as part of this project, subject to cost participation.

- 5) What are the specific plans for the north Krueger Road intersection and is a pathway connection (stub) anticipated for a future tie-in along North Krueger Road?

Response: The improvement of the IL 22 and North Krueger Road intersection would require the reconstruction of North Krueger Road to approximately 500 feet north of IL 22 to Krueger Court. The proposed scope of work includes one traffic lane in each direction along North Krueger Road, a traffic signal, and realignment of Blackhawk Lane directly across from North Krueger Road. The proposed curb and gutter along IL 22 would transition to shoulders along North Krueger Road. Drainage swales would be provided along each side of the roadway. Proposed retaining walls have been minimized at this intersection as requested by public meeting comments. Retaining walls are still required along the west side of North Krueger Road between IL 22 and Krueger Court to avoid encroachment into a SCE. Currently no path is proposed along North Krueger Road, however, the intersection is being designed to accommodate future pedestrian/bicycle crossings. For additional details, see the attached Preliminary Plan & Profile and Typical Section for North Krueger Road.

There was concern about the 20% match associated with this proposal and where those funds would come from implementation/completion of this project. The consensus of the Committee was to be aware of the match requirement to deal with the funding issue in the future.

Response: At the end of Phase I, a Letter of Intent will be sent the Village which will estimate the Village's cost participation and maintenance requirements.

Recommendation unanimously approved by the Long Grove Pathway Committee

2.18.11 - Regular Meeting

~ Draft ~
**IDOT/VILLAGE OF LONG GROVE
COORDINATION MEETING MINUTES
Tuesday, May 4, 2010
Village of Long Grove, Village Hall**

**Illinois Route 22
Quentin Road to Illinois Route 83
P-91-284-00**

The purpose of the meeting was to update the Village on the comments received following the Public Information Meeting (PIM) and to present a revision to the proposed plan that addresses the comments. Village input on the plan revision is requested in select areas. The PIM was attended by 95 people, and 45 written comments were received. A Public Hearing, near the end of the Phase I process, is planned for late 2010. Once Village input is received, IDOT will proceed with finalizing the recommended plan.

IDOT met with the Village of Kildeer and the Lake County Forest Preserve District (LCFPD) to present how their comments have been addressed. A brief review of those changes was presented. Kildeer requested that the proposed 5-foot sidewalk be replaced with a 10-foot sidepath, which would extend from a planned Lake County Division of Transportation (LCDOT) path along Quentin Road east to Old McHenry Road. The sidepath will likely be bituminous and located five feet behind the back of curb on the south side of IL Route 22. Kildeer and LCFPD may pursue paths away from the roadway, within Egret March F.P. and Heron Creek F.P. connecting the two forest preserves, however the roadway plans will include a shelf that will accommodate the desired path within the roadway right-of-way. With the addition of the sidepath to the plans, the width of the outside lane in Kildeer is proposed to be reduced from 14 feet to 12 feet.

LCFPD had requested that any swales on or adjacent to their property be natural-looking, with a meandering alignment, areas of ponding, and rocks. LCFPD had stated that tree replacement would be allowed in the forest preserves in areas that would be designated as temporary use areas. These areas would be outside of the easements and right-of-way required to construct the road. Long Grove was not opposed to tree replacement being added to the forest preserves, however would prefer that trees be replaced near where they are removed from whenever possible.

Long Grove stated that they have prepared plans for a path along the west side of Old McHenry Road from IL Route 22 to downtown. The Village will provide these plans to IDOT.

The two primary Village of Long Grove comments subsequent to the PIM were requests to reduce/eliminate right-of-way impacts and minimize retaining walls. This is a significant challenge, as many of the retaining walls are required to specifically prevent encroachment into the Village's scenic corridor easements (SCE's). IDOT has spent the past several months making adjustments to the roadway alignment, profile, and retaining wall offsets to address the Village's most recent comments, to balance the Village's aesthetic concerns with IDOT's need to provide a safe and efficient roadway improvement.

Regarding retaining walls, there are several locations at which walls cannot be removed from

the plans, such as adjacent to high-quality wetlands and at floodplains. Also, there are some locations where walls have been added to the plans due to alignment and profile shifts, the need to provide sufficient room for drainage features and utility relocations, and also the need to maintain existing berm heights. There are several locations where retaining walls were able to be removed, shortened, or lowered. It was stated that there are still several locations where walls are shown to prevent or limit encroachment onto private property or SCE's. Further reductions in the number and extent of walls can be accommodated if the Village is agreeable to a certain degree of temporary or permanent easements within SCE's.

Regarding impacts to private property and the SCE's, there are locations at which temporary easements (TE's), permanent easements (PE's), and proposed right-of-way (PR ROW) will be unavoidable no matter what design is shown. Temporary easements will be required in select locations for the following reasons:

- Grading of embankments or cut sections to meet the existing ground.
- Re-establishment of driveways and cross-roads.
- Need to provide area for construction of box culverts perpendicular to IL Route 22.
- Need to provide 10 feet of space behind retaining walls for construction.
- Areas where retaining walls will need to be shortened or tapered to provide for sight distance at driveways and cross-streets.

Permanent easements or right-of-way acquisition will be required in select locations for the following reasons:

- Placement and maintenance of traffic signal equipment.
- Maintenance of box culverts.
- Maintenance of drainage swales.
- Compensatory storage to account for fill in floodplains.
- Roadway construction in extremely constrained sections.
- Areas where retaining walls will need to be shortened or tapered to provide for sight distance at driveways and cross-streets.
- A minimum of 7 feet of right-of-way or permanent easement is required behind retaining walls for placement of swales and utilities.

Plan and Profile sheets were presented, showing the changes to the design that address the Village's request to minimize retaining walls and property impacts. Changes to the proposed alignment and profile since the PIM were shown in red on the plans. Retaining walls that have been removed were shown with red "X's" on them. In locations where retaining walls could be removed if the Village was agreeable to encroachment into SCE's or private property, green linework was shown that illustrated the resulting grading limit line and any required temporary or permanent easements. It is at these locations that IDOT requests the Village to select the preferred course of action: retaining walls or no retaining walls with some encroachment.

The Village re-stated a concern about the aesthetic character of the walls. The Village requested that the walls be both durable and low-maintenance, and asked if modular block walls could be used. IDOT responded that those are acceptable when the wall is retaining the adjacent ground, i.e., in cut-sections where the adjacent ground is higher than the roadway.

This type of wall is also only feasible when the wall is set a sufficient distance from the roadway. There is a height limitation for the installation of this type of wall. The Village prefers decorative walls to plain concrete where possible. A photo of a block wall installation on IL Route 22 to the east was shown, and is attached.

Referring to the plans, Civiltech began at the western Village boundary and explained the reason for the retaining walls, noted where they were added or removed from the plans, and also noted where the Village has an option to have a retaining wall installed or not. Walls are numbered on the plan sheets for reference.

- **Wall #8.1, Sta. 79+50, LT - ADDED** – Just west of the Village border, the alignment was shifted north to reduce residential and business relocations. This new fill-section retaining wall is shown to avoid impacts to the ADID wetland on the north side of IL Route 22. IDOT may determine that this wall could be removed from the plans with mitigation for the slight impact to the wetland.
- **Wall #10, Sta. 108+00, LT - SHORTENED** – The length of the proposed cut-section wall has been reduced at the farm property on the northeast corner of IL Route 22/Old McHenry Road at the request of the property owner. The owner prefers grading onto the property in lieu of retaining walls. The remaining cut-section wall could be removed from the plans if redevelopment of this property pre-dates the roadway improvement.
- **Wall #11, Sta. 110+00, RT - SHORTENED** – This fill-section wall was able to be shortened on both ends, however most is still required to avoid impacts to the SCE. If the wall is constructed, a TE will be required in the SCE to allow for 10 feet of room behind the wall for construction. It was noted that for roadside safety, each of the walls along the corridor will need to be protected in some fashion through the use of guardrails or impact attenuators extending beyond the limits of the physical wall, at both ends. Illustrations of these treatments were shown and are attached. It was also noted that a box culvert exists at Sta. 113+00, which will require TE and PE in the SCE under any option.

Village Option: This wall can be removed if the Village allows additional PE in the SCE. The PE would be required to re-establish an existing drainage swale that gathers runoff from private property toward the roadway and runoff from the roadway embankment behind the back of curb. Short segments of the existing path would also be re-graded within the PE. IDOT will discuss with its hydraulics and right-of-way staff the potential for the swale to be Village-maintained, which might eliminate the need for the PE. Only a TE would be required.

- **Wall #12, Sta. 111+00, LT - REMOVED** – This cut-section wall has been removed at the farm property on the northeast corner of IL Route 22/Old McHenry Road at the request of the property owner. The owner prefers grading onto the property in lieu of this wall.
- **Wall #13, Sta. 118+00, LT - REMOVED** – This cut-section wall has been removed. Grading is now proposed within the PR ROW. PR ROW is required under any roadway improvement scenario and cannot be avoided. There is no SCE present at this

location.

- **Wall #14, Sta. 119+00, RT - REMOVED** – This cut-section wall has been removed. Roadway grading can be accommodated within the existing right-of-way.
- **Wall #15, Sta. 120+00, RT - SHORTENED** – This fill-section wall was shortened on the east end by allowing some drainage to the roadway over the back of curb since the roadway profile is in a crest section. The remainder of the wall must be kept to avoid encroachment into the SCE. Even with the wall, PE and TE will be required in the SCE at the Oak Creek Lane to allow for a tapered wall for sight distance. Aside from the sight distance taper, this wall will be constructed at the back of the 2-foot concrete gutter along the south edge of pavement. To provide for necessary drainage swales and utilities replacement, it will be a Jersey-style concrete barrier wall, similar to what is constructed on IL Route 22 east of IL Route 21 in Lincolnshire. It will require end protection in the form of impact attenuators or guardrail extending from both ends. See attached photos.

Village Option: This entire wall can be eliminated if the Village allows a PE in the SCE. The PE would be required to establish a drainage swale to gather both runoff from private property toward the roadway (in most sections) and runoff from the roadway embankment behind the back of curb (in all sections). IDOT will discuss with its hydraulics and right-of-way staff the potential for the swale to be Village-maintained, which might eliminate the need for the PE. It was noted that there would be minimal tree loss within this potential PE as there are few trees behind the proposed edge of pavement between Oak Creek Lane and Sta. 124+00. IDOT also noted that if the Village were to allow an additional temporary use area within the SCE, this would be a prime location to plant replacement trees.

Mr. Lothspeich indicated he soon had to leave for another appointment, so subsequent wall numbers 16, 16.1, 17, 18, 18.1, 19, and 22 were not discussed in detail. There are Village options at wall numbers 16, and 18.1. It was noted that the acquisition previously shown on the Kapov property on the northwest corner of IL Route 22 and North Krueger Road is no longer needed due to an alignment shift to the south over what was previously proposed, and a small raising of the profile in that area. The following walls were briefly discussed:

- **Walls #20 and #21, Sta. 141+00, RT/LT - LOWERED** – These fill-section walls must remain due to ADID wetlands, and floodplains and floodways associated with Willowbrook Drain. The walls also minimize impacts near the Eastern Prairie Fringed Orchid Nature Preserve. However, the walls have been lowered by reducing IDOT's minimum freeboard requirement to below 3 feet above the 5-year storm at the edge of pavement. The profile is shown below what was previously proposed, though it is still about two feet above the existing centerline grade. This will require a design exception.
- **Wall #22.1, Sta. 151+00, RT - ADDED** – This cut-section wall was added to the plans to preserve the height of the existing berm, and to prevent encroachment into the SCE.

Village Option: All or portions of this wall could be removed if the Village allowed

temporary easements for grading into the adjacent SCE. A reduction in berm height of 1 to 3 feet would result.

- **Wall #23, Sta. 161+00, RT - NO CHANGE** - This wall contains both cut- and fill-sections. It protects existing berm height and minimizes encroachment in to the adjacent SCE. This wall must be set so that it does not interfere with sight distance at Old Indian Creek Lane or Stone Haven Drive. A TE will be required in the SCE to construct this wall, and a PE will be required near Stone Haven Drive to accommodate sight distance. A Jersey-style concrete barrier at the back of gutter is not feasible due to sight distance needs. A TE and PE are also required to construct and maintain the box culvert crossing at approx. Sta. 162+75.

Village Option: All or portions of this wall could be removed if the Village allowed TE's for grading and a section of PE for a drainage swale in the adjacent SCE. A 1 to 2-foot reduction in berm height would result along a 100-foot length of berm. IDOT will discuss with its hydraulics and right-of-way staff the potential for the swale to be Village-maintained, which might eliminate the need for much of the PE. Only at TE would be required. A PE will still be required at the box culvert crossing.

- **Wall #24/25, Sta. 166+00, RT - LENGHTENED** - This cut-section wall was not discussed in detail. Its purpose is to maintain existing berm heights. A TE is required in the SCE to construct this wall. The eastern portion is Jersey-style barrier at the back of gutter.

Village Option: All or portions of this wall could be removed if the Village allowed TE's for grading PE's for a drainage swale in the adjacent SCE. A reduction in berm heights would result in some sections. IDOT will discuss with its hydraulics and right-of-way staff the potential for the swale at 169+00 and 170+00 to be Village-maintained, which might eliminate the need for the PE. Only a TE would be required.

- **Walls Along North Krueger Road and Krueger Court** - The existing slope of North Krueger Road is 6% down to IL Route 22. The proposed slope of North Krueger Road was previously shown as improved to 4%. Many PIM attendees were pleased that the slope was proposed to be reduced. The improvement to 4% required a number of retaining walls along North Krueger Road and Krueger Court. At the Village's request, in an effort to reduce retaining walls and reduce the slope of private Krueger Court, the slope of the proposed profile was changed back to 6%, which is no better than the existing slope. The incline would be shifted approximately 100 feet north of its current location, away from IL Route 22. The profile change is illustrated on the plan sheet. Several walls were able to be removed from the plan and Krueger Court grade will be lessened. A shift in the alignment of IL Route 22 to the south (five feet more than the previous design), and a raising of the profile (one foot more than the previous design) helped the reduction of walls. Civiltech noted that the 6% grade approaching a signalized intersection is not preferable. IDOT noted that the design still needs IDOT Geometrics approval, so is not final.

The Village expressed an understanding of how grading impacts into a SCE may be preferred over the use of retaining walls in some areas. However, any impacts to the SCE's will need to be approved by the Village Board for the project to receive Village support. The Village is

generally opposed to transferring ownership of the SCE's. IDOT explained that, where the only impact is grading back to existing ground, a temporary construction easement would be needed, in which the ground would be graded, and ownership of the land would not change. The Village requested that, if the ground is graded, it should be flat enough to allow for plantings. IDOT stated that, if a swale is needed outside the right-of-way to capture water draining from off-site toward the road, a permanent easement would be needed to allow IDOT to maintain the swale. However, IDOT stated that if the Village agrees to maintain the swales, a permanent easement may not be needed. IDOT will verify with its Hydraulics and Right-of-Way groups to determine if this is possible. The Village asked if drainage could be carried in a pipe rather than a swale to reduce impacts. IDOT responded that a swale would still be needed to capture the runoff before it could be drained into a pipe.

Other items subsequently discussed with the Village included the following.

- Due to tree impacts, IDOT has elected to remove the previously proposed water quality basin from the plans. A combination of in-line detention, mechanical separators and long ditches to improve water quality prior to runoff entering Willowbrook Drain will be investigated instead.
- As a certified community, the Village asked if the Village engineer's approval for the drainage plans would be required. IDOT stated it is working to meet the spirit of the Lake County ordinance, however only IDOT approval is required.
- The Village pointed out existing drainage problem areas:
 - The Heron Creek pond has overtopped the roadway on occasion. Additional fill in pond may make it worse. IDOT noted that the flood elevation is not known, as this is listed as a Zone A floodplain on the FEMA maps, which do not include 100-year flood elevation. IL Route 22 is being raised here to prevent pavement flooding, and a report is in the process of being prepared for the nearby box culvert.
 - The Willowbrook Farm pond on south side of IL Route 22 approx. Sta. 151+00 is a man-made pond that causes flooding problems for the adjacent homeowners. This pond does not overtop IL Route 22.
 - Flooding problems were noted near the Royal Melbourne pump house on north side of IL Route 22 approx. Sta 162+00. A report is in the process of being prepared for the nearby box culvert.
- Since right-of-way is not available east of Old McHenry, the 10-foot bituminous sidepath cannot be continued. A path could be continued if construction in the SCE's was allowed. The Village stated it is considering a path east to North Krueger Road, and then north to Gilmer Road, where it could connect to two schools. At the Village's request, IDOT is attempting to not preclude the future installation of a path along North Krueger Road. The Village will state to IDOT its preference for or against the construction of a path as part of the IL Route 22 roadway improvement.
- The Village noted that its tree replacement policy for signature trees is on an inch-by-inch basis, and replacements must be three inches minimum. For example, a 24-inch tree would be replaced by eight 3-inch trees, six 4-inch trees, etc. IDOT's tree replacement policy is a 1:1 basis. The Village forester would be on-site to oversee the

planting.

- North Krueger Road is planned to be closed between IL Route 22 and Krueger Court during the reconstruction of this segment due to the profile corrections along North Krueger Road combined with the constrained working area. A preliminary detour plan has been prepared to route traffic north to Gilmer and back south along IL Route 83.
- Following the Public Meeting, the installation of a temporary traffic signal was requested at the intersection of IL Route 22/North Krueger Road until the permanent traffic signal is completed as a part of the proposed roadway improvements. This was studied and determined to not be feasible since, without left turn lanes and sufficient roadway capacity on IL Route 22, a traffic signal would be a detriment to the operation of IL Route 22. A permanent traffic signal will be part of the proposed roadway improvements, despite the term "potential" used in the PIM brochure.
- The Village requested a copy of the plans as presented. IDOT stated that these revised plans have not been through thorough review in the Bureau of Design, therefore they cannot be provided at this time. IDOT will provide the Village a less engineered exhibit in PDF format for presentation to the Board, along with other exhibits and a list of talking points. IDOT offered to make the presentation to the Village Board. The Village and IDOT will discuss the possibility of a presentation at the May 25th meeting.

The following commitments were made:

- The Village will provide plans to IDOT for the proposed path along the west side of Old McHenry Road to IDOT.
- IDOT will internally discuss the possibility of the Village maintaining drainage swales outside of the roadway right-of-way if this condition would eliminate the need for permanent easements within the scenic corridor easements. Temporary easements would still be required in most locations.
- The Village will coordinate with IDOT about the possibility of an IDOT presentation at a Village Board meeting.
- After the Board meeting, the Village will provide a written summary of its preferences for or against retaining walls at the identified locations where there is an option.
- The Village will state to IDOT its preference for or against the construction of a path as part of the IL Route 22 roadway improvement.

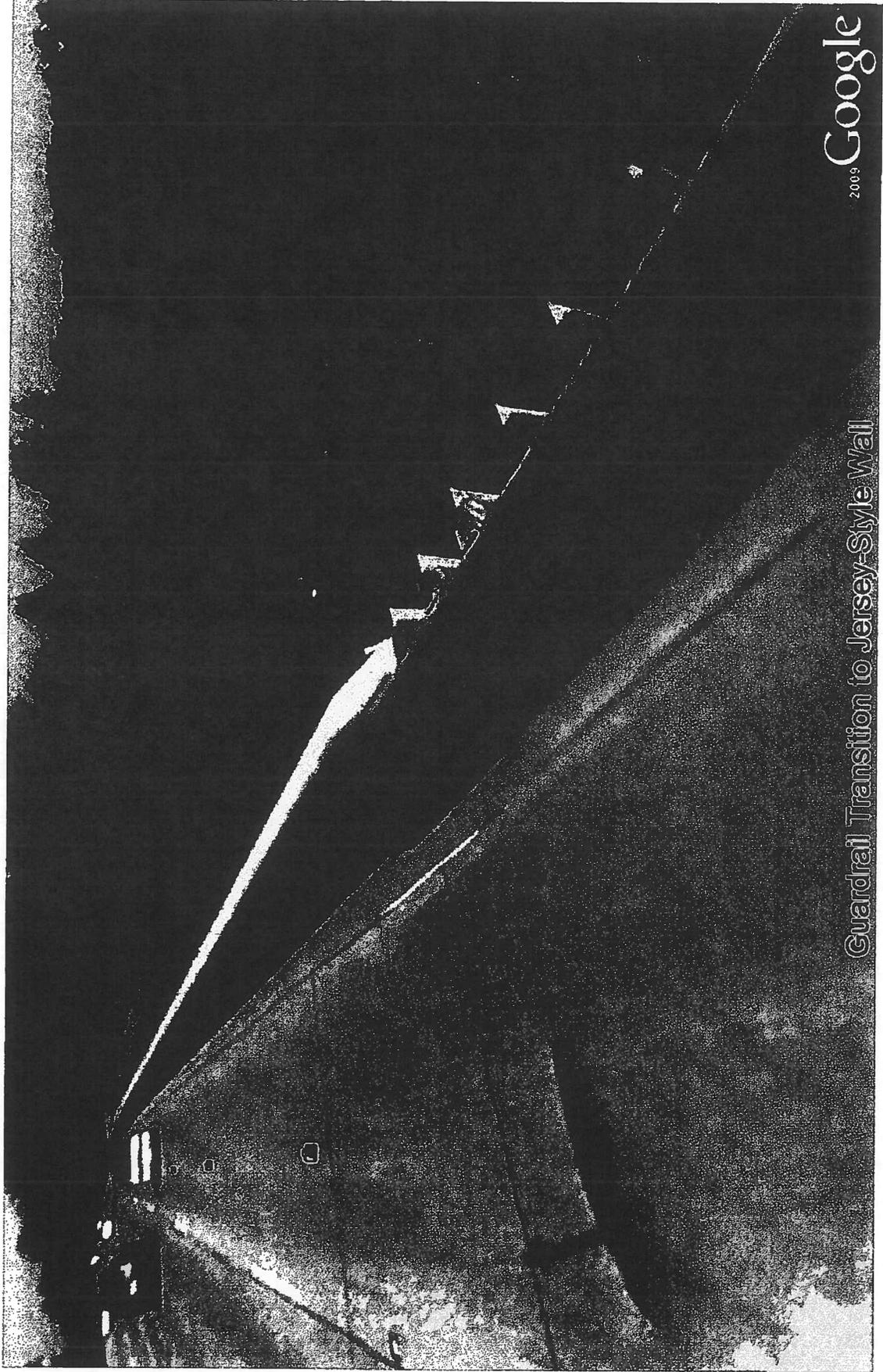
By:

James R. Woods, P.E., PTOE and Joseph J. Emry, P.E., Civiltech Engineering, Inc.



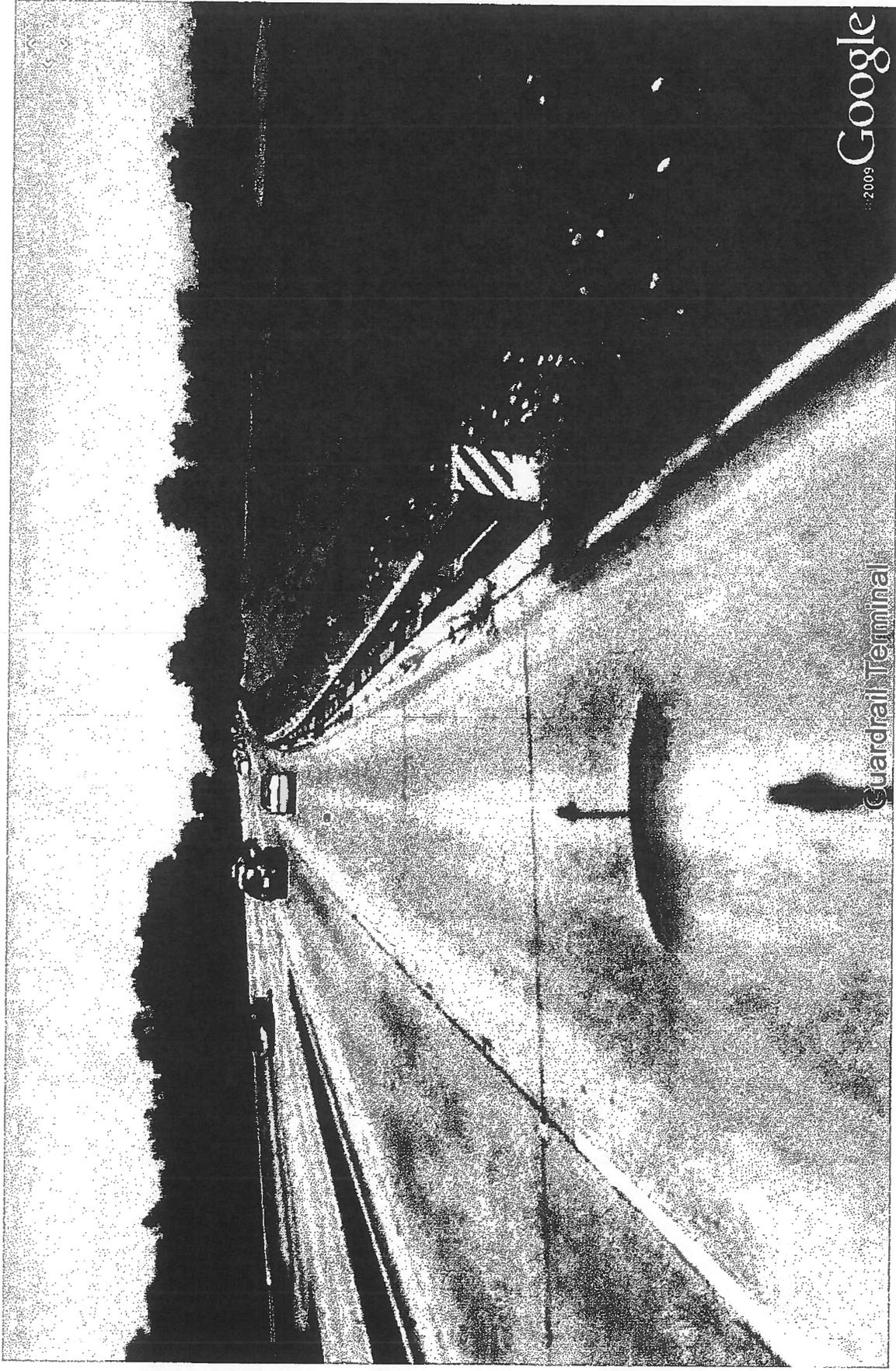
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Impact Attenuator at End of Retaining Wall



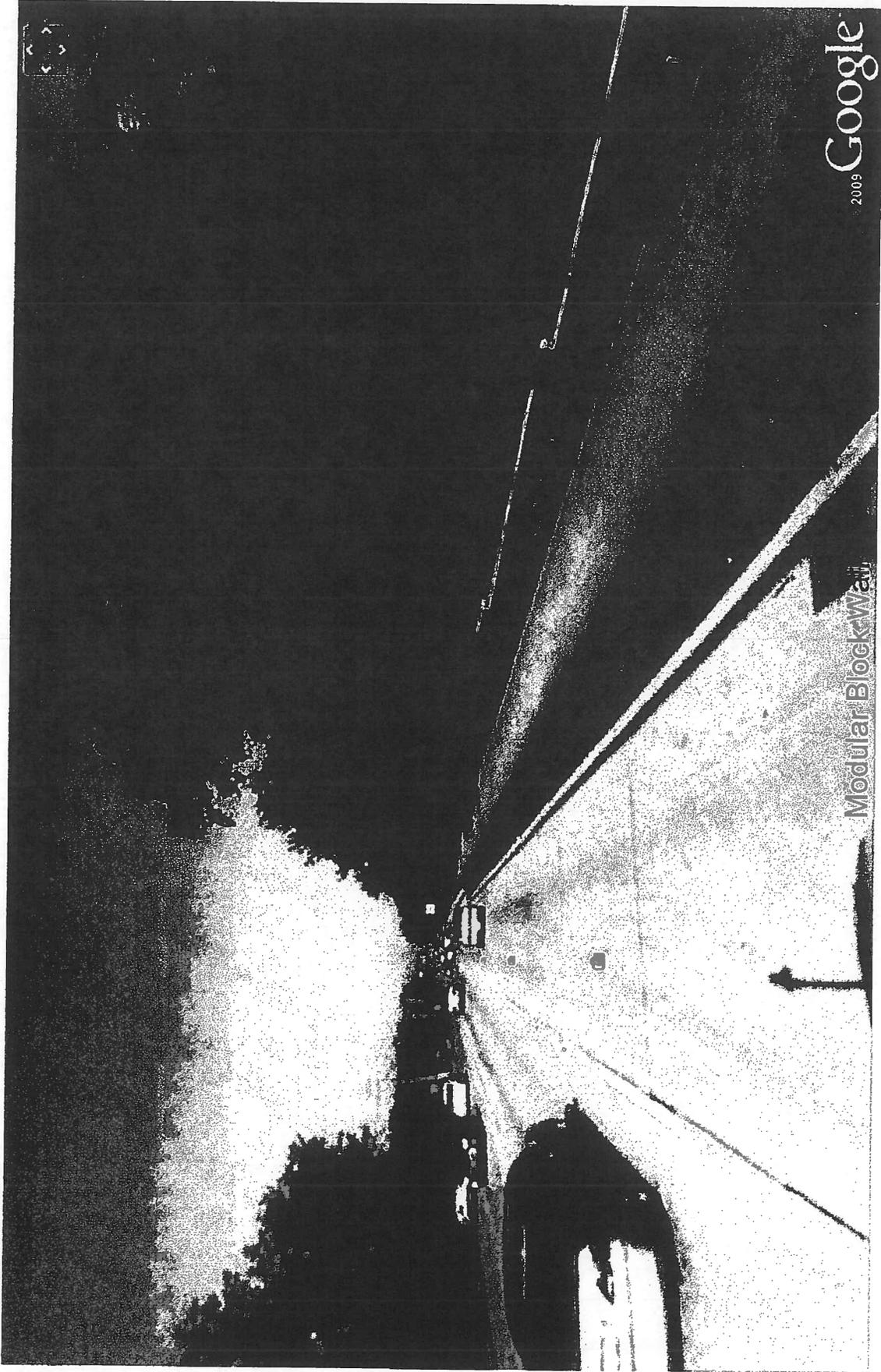
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Guardrail Transition to Jersey-Style Wall



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Guardrail Terminal



2009 Google

Modular BlockWay

Illinois Route 22
Quentin Road to IL Route 83
P-91-284-00

**Recommended Improvement Plan – Final Modifications
Presentation to Village of Long Grove Board**

Executive Summary

- A Public Information Meeting was held in November of 2009, and comments were received. The Village's primary comments were to further minimize the amount of property acquisition, primarily in Village scenic corridor easements, and also to further minimize the number and extent of retaining walls.
- IDOT has adjusted the proposed design based on public and Village comments and has prepared a revised plan that balances the aesthetic concerns of the Village with IDOT's need to provide a safe and efficient roadway improvement.
- The plan as it is presented now represents the pre-final recommended improvement plan. There are select locations at which IDOT is requesting Village input. Once received, IDOT will finalize the recommended improvement plan with internal reviews, complete drainage studies, complete the environmental and design reports, and conclude the Phase I Engineering stage of the improvement project.
- With respect to temporary or permanent easements, there are several locations at which they will be unavoidable in scenic corridor easements for several reasons.
- With respect to retaining walls, there are several locations at which IDOT has been able to remove retaining walls from the proposed plan by further adjusting the roadway alignment, profile and cross-section. There remain several locations within Long Grove at which walls cannot be removed from the plans for environmental reasons.
- There are also several walls remaining in the proposed plan whose function is solely to prevent or minimize temporary or permanent acquisition in scenic corridor easements. Removal of these "optional" walls from the plans can be accommodated if the Village is agreeable to a certain degree of additional temporary or permanent easements within the scenic corridor easements.
- There are 12 retaining walls proposed within the Village, totaling 5,500 feet in length. Seven of the twelve, totaling 3,400 feet in length, are considered "optional".
- There are several safety and aesthetic benefits of grading into the scenic corridor easements versus constructing the optional retaining walls.
- IDOT requests that the Village state its preference for or against each individual retaining wall that is considered "optional".
- Attachment A contains additional details on the above points. Attachment B is a roll-out plan view on aerial. Attachment C is a matrix summarizing the property impacts of the retaining walls. Attachment D is a collection of colorized perspective views illustrating a typical section containing an optional retaining wall.

Attachment A – Additional Details

Illinois Route 22
Quentin Road to IL Route 83
P-91-284-00

**Recommended Improvement Plan – Final Modifications
Presentation to Village of Long Grove Board**

Attachment A – Additional Details

- A Public Information Meeting was held in November of 2009. Comments were received from residents and local agencies. IDOT has spent the past several months adjusting the proposed design and has prepared a revised recommended plan that addresses the comments received. The revised plan balances the aesthetic concerns of the Village with IDOT's need to provide a safe and efficient roadway improvement.
- The primary comments from the Village of Long Grove were to minimize the amount of property acquisition, primarily in Village scenic corridor easements (SCE's), and also to minimize the number and extent of retaining walls. This was a considerable challenge. A reduction in retaining walls inherently results in additional encroachment into scenic corridor easements. Conversely, minimizing impacts in the scenic corridor easements necessitates the construction of retaining walls.
- The plan as it is presented now represents the pre-final recommended improvement plan. There are a number of specific locations at which IDOT is requesting Village input. Once received, IDOT will finalize the recommended improvement plan with internal reviews, complete drainage studies, complete the environmental and design reports, and conclude the Phase I Engineering stage of the improvement project.
- Attachment B is a roll-out aerial plan view exhibit. Following is an overview. Details are explained later.
 - The plan view shows the improvement limits within the Village of Long Grove, from east of Salem Lake Drive to IL Route 83.
 - The proposed pavement is shown in grey, and pavement markings are shown in yellow and white.
 - The existing roadway centerline is shown as a thin black line.
 - The previously proposed roadway centerline as presented at the November 2009 Public Meeting is shown in black, and includes roadway stationing.
 - The red centerline shown in some locations illustrates the proposed alignment shifts that have been made to help address the Village's comments.
 - The orange dashed line is the existing roadway right-of-way line.
 - The brown hatched line shows Village scenic corridor easements.
 - Proposed easements and right-of-way are shown as black dashed lines. Some are temporary and some are permanent. They are labeled as such.
 - Proposed retaining walls are shown as thick black or red lines. The black retaining walls are required, and the red retaining walls are optional. An explanation of the optional walls will be provided later.
 - Easements that would be required if the optional walls were not installed are shown as blue dashed lines. Some are temporary, some are permanent. These will also be discussed later.

Temporary and Permanent Easements

- With respect to temporary or permanent easements, there are several locations at which they will be unavoidable in scenic corridor easements. These are shown on the exhibit as black dashed lines outside of the existing right-of-way. They are labeled either "T/E" for "temporary easement", or "P/E" for "permanent easement."
 - Temporary Easements will be required in select locations for one or more of the following reasons:
 - Re-establishment of driveways and cross-streets.
 - Grading to meet the existing ground.
 - Construction area around major box culverts.
 - Space behind retaining walls for construction (ten feet minimum).
 - Permanent Easements will be required in select locations for one or more of the following reasons:
 - Placement and maintenance of traffic signal equipment.
 - Maintenance area around major box culverts.
 - Construction and maintenance of drainage swales. Some of these Permanent Easements could possibly be only Temporary Easements if the Village agrees to maintain swales that collect non-roadway runoff.
 - Compensatory storage to account for fill in floodplains (none in SCE's).
 - Roadway elements in extremely constrained sections (none in SCE's).
 - Areas where retaining wall ends are tapered for sight distance at drives and cross-streets.

Retaining Walls

- There are several locations within Long Grove at which walls cannot be removed from the plans, such as adjacent to high-quality wetlands, at floodplains and stream crossings, and to protect existing buildings. These are shown on the aerial exhibit as thick black lines.
- There are some locations at which walls have been added since the public meeting due to the need to provide room for construction, drainage swales and utility relocations. Other walls have been added to preserve existing berm heights.
- Finally, there are several locations at which IDOT has been able to remove retaining walls from the proposed plan that was shown at the public meeting. This was accomplished by shifting the proposed centerline of the roadway, raising or lowering the proposed profile of the roadway, and by shifting the lateral placement of the retaining walls adjacent to the roadway. This was a lengthy, iterative process that has reached the point of diminishing returns.
- In addition to the retaining walls required for wetland and floodplain protection, several walls remain within the proposed plan whose function is solely to prevent or minimize temporary or permanent acquisition in scenic corridor easements. Removal of these "optional" walls from the plans can be accommodated if the Village is agreeable to a certain degree of additional temporary or permanent easements within the scenic corridor easements.
- There are 12 retaining walls proposed within the Village. Seven of the twelve are considered "optional". See the Table, provided as Attachment C, for a summary of property impacts with and without the optional retaining walls.

Rendered Typical Section Exhibits

- Attachment D includes rendered typical section exhibits that show a sample location where an optional retaining wall is proposed, to illustrate the future conditions with and without the optional wall. Optional Wall #15 is shown as an example. The viewpoint is near Oak Creek Lane at Sta. 119+00, looking east.
 - Exhibit D-1 shows existing conditions, "View 1".
 - Exhibit D-2 shows the proposed conditions from "View 1", with the optional retaining wall. This wall will require a concrete Jersey-style barrier as shown, two feet from the edge of pavement, similar to those found on IL Route 22 in Lincolnshire. The actual retaining wall is located behind this barrier wall. Note that most barriers and retaining walls will require end protection in the form of an impact attenuator (shown) or guardrail.
 - Exhibit D-3 shows the proposed conditions from "View 1", without the optional retaining wall. Due to the constraints of the scenic corridor easement, there is no room for tree replacement.
 - Exhibit D-4 shows the proposed conditions from "View 1", without the wall, however this condition assumes that the Village allows IDOT to plant replacement trees within the scenic corridor easement.
 - Exhibits D-5 through D-8 show the same area as viewed from a point on Oak Creek Lane. The back of the concrete barrier wall is shown. Below is the actual retaining wall. A decorative concrete form liner could be used on this back face of the retaining wall.
 - Exhibit D-9 shows "View 3", the proposed conditions with the optional retaining wall, a bit further east on IL Route 22. It shows the concrete Jersey-style barrier from a motorist's perspective.

Benefits of Grading in SCE vs. Constructing a Retaining Wall

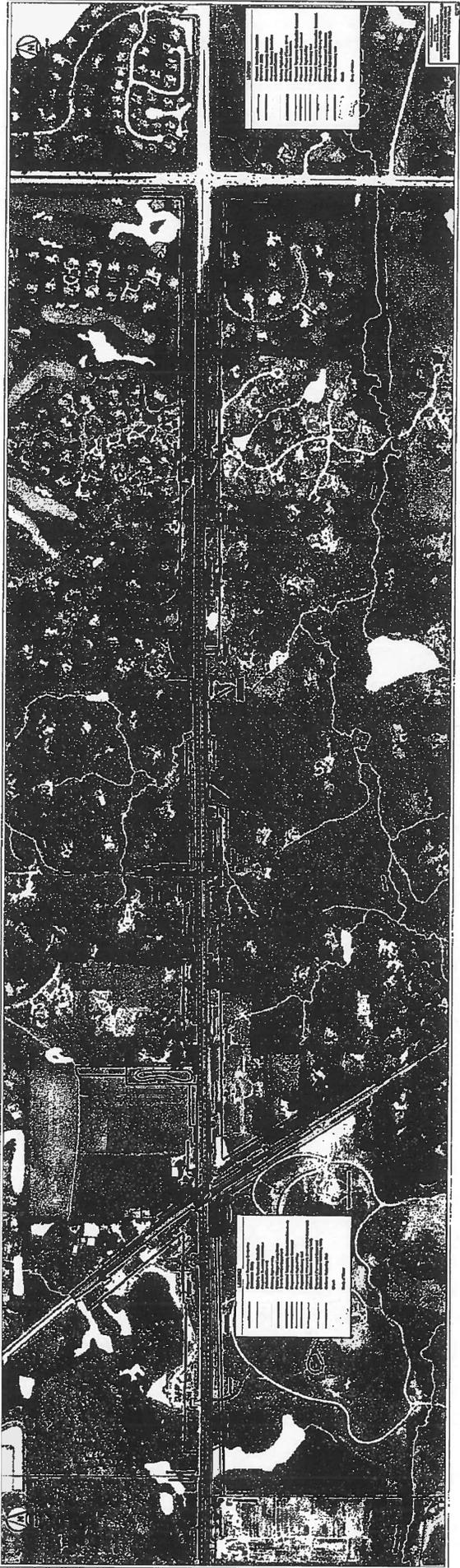
- Aesthetics - Grading results in a natural landscaped appearance vs. retaining walls and required end protection elements. As the retaining wall ages, appearance declines. The area between the back of curb and the wall can be difficult to maintain, and may trap loose trash.
- Safety – Retaining walls, Jersey barriers, and end sections are roadside hazards. Jersey barriers at the face of curb provide no refuge area for stranded motorists or bicyclists, and can inhibit snow removal efforts. Retaining walls near driveways and cross-streets can be sight distance hazards.
- No roadway appurtenances would be in the SCE. The ground would be restored.
- Invasive trees can be removed and signature trees can be planted. IDOT's landscape architect can meet with Village staff this summer to develop a concept plan.
- In a number of the areas with optional retaining walls, only temporary easements would be required during construction. This is especially true if the Village agrees to maintain swales behind the wall that collect non-roadway runoff.
- Utility poles may stay within the existing roadway right-of-way.

Summary

Twelve retaining walls totaling 5,500 feet in length are proposed along the IL Route 22 project within the Village. Seven of these walls, totaling 3,200 feet in length, are considered optional. The optional walls protect a total of only four trees whose species are listed on the Village's protected tree list. At four of the seven optional retaining walls, temporary and/or permanent easements will be required anyway if a retaining wall is constructed. IDOT requests that the Village state its preference for or against each individual retaining wall that is considered "optional".

Attachment B – Plan View Exhibit on Aerial Photo

6/16/2010



Attachment C – Retaining Walls in the Village of Long Grove

PROPOSED RETAINING WALLS IN LONG GROVE

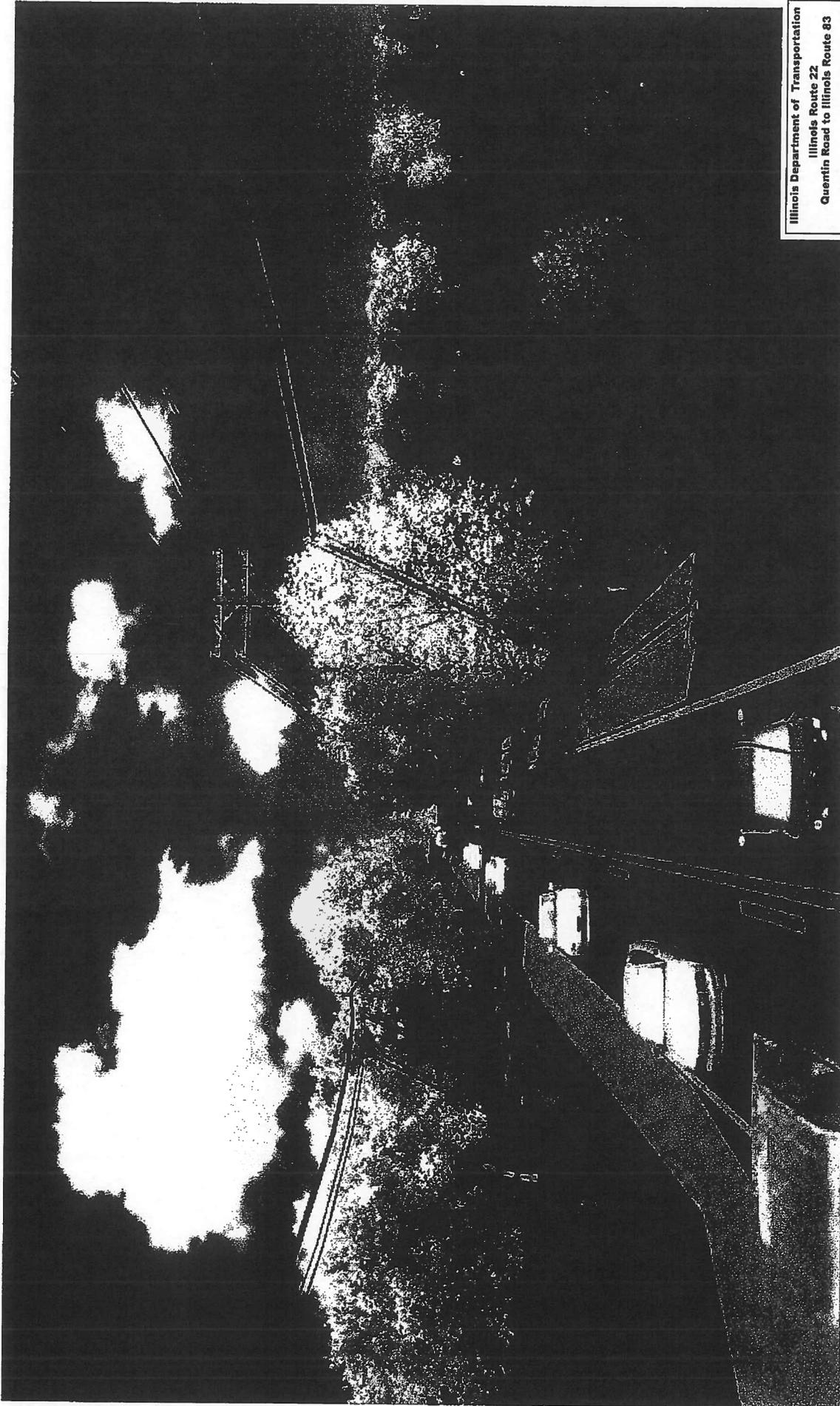
June 16, 2010

Ref. No.	Optional	Wall Location		Length (feet)	Section Type ⁽¹⁾	Offset from back of curb	Height Range ⁽²⁾		Reason for Wall	SCE Impacts With Optional Wall	SCE Impacts If Optional Wall Removed	Approx. No. Trees Saved w/Retaining Wall ⁽³⁾
		Roadway	Station/ Side				Roadway Side	Opposite Side				
9	No	IL 22	87+00 S	350	Fill	17.5'	3.5'	8'	Limit fill in Heron Creek Forest Preserve pond	-	-	-
10	No	IL 22	109+00 N	250	Cut	10'	4' to 7'	3.5'	Preserve residential buildings.	-	-	-
11	Yes	IL 22	112+00 S	350	Fill	10'	3.5'	5' to 8'	Limit encroachment in SCE.	T/E required in SCE to construct wall.	P/E required in SCE for drainage swale construction and maintenance. ⁽⁴⁾	0/5
15	Yes	IL 22	121+00 S	400	Fill	at face of curb to 10'	3.5'	5' to 6'	Limit encroachment in SCE.	T/E required in SCE to construct wall. P/E required in SCE to accommodate end of wall.	P/E required in SCE for drainage swale construction and maintenance. ⁽⁴⁾	0/1
16	Yes	IL 22/ N. Krueger	130+00 N	650	Cut	at face of curb (IL 22) to 18' (NK)	3.5' (IL 22) to 13' (NK)	3.5'	Prevent encroachment in SCE.	No additional T/E or P/E required.	T/E required in SCE to grade up to existing ground.	4/34
18.1	Yes	IL 22	133+00 S	100	Fill	10'	3.5'	9'	Prevent encroachment in SCE.	No additional T/E or P/E required.	T/E required in SCE to grade down to existing ground.	0/4
19	No	IL 22	137+00 S	200	Fill	10'	3.5'	8' to 10'	Prevent impacts to creek.	-	-	-
20	No	IL 22	144+00 S	800	Fill	10'	3.5'	8' to 14'	Minimize impacts to ADID wetlands, floodplain, floodway, Willowbrook Drain, Orchard Preserve.	-	-	-
21	No	IL 22	144+00 N	500	Fill	10'	3.5'	8' to 10'	Minimize impacts to ADID wetlands, floodplain, floodway, Willowbrook Drain.	-	-	-
22.1	No & (Yes)	IL 22	153+00 S	200 & (300)	Cut	7'	8' to 10'	3.5'	Prevent encroachment in SCE, preserve existing berm heights & (maintain separation from pond).	No T/E or P/E required in SCE.	T/E required in SCE to grade up to existing ground. Berm reduced by up to 1.5 feet.	0/14
23	Yes	IL 22	162+00 S	350	Cut/Fill	7'	3.5' to 8'	3.5' to 8'	Limit encroachment in SCE, preserve existing berm heights.	T/E required in SCE to construct wall. P/E required in SCE to accommodate end of wall.	T/E required in SCE to grade up to existing ground. Berm reduced by about 1.5 feet. P/E req'd in SCE for swale const. and maint. ⁽⁴⁾	0/5
24	Yes	IL 22	170+00 S	1,050	Cut/Fill	at face of curb to 7'	3.5' to 9'	3.5' to 8'	Limit encroachment in SCE, preserve existing berm heights.	T/E required in SCE to construct wall.	T/E required in SCE to grade up to existing ground. Berm reduced by up to 2 feet. P/E req'd in SCE for swale const. and maint. ⁽⁴⁾	0/49
Totals				5,500								
				Optional	3,200							4/112

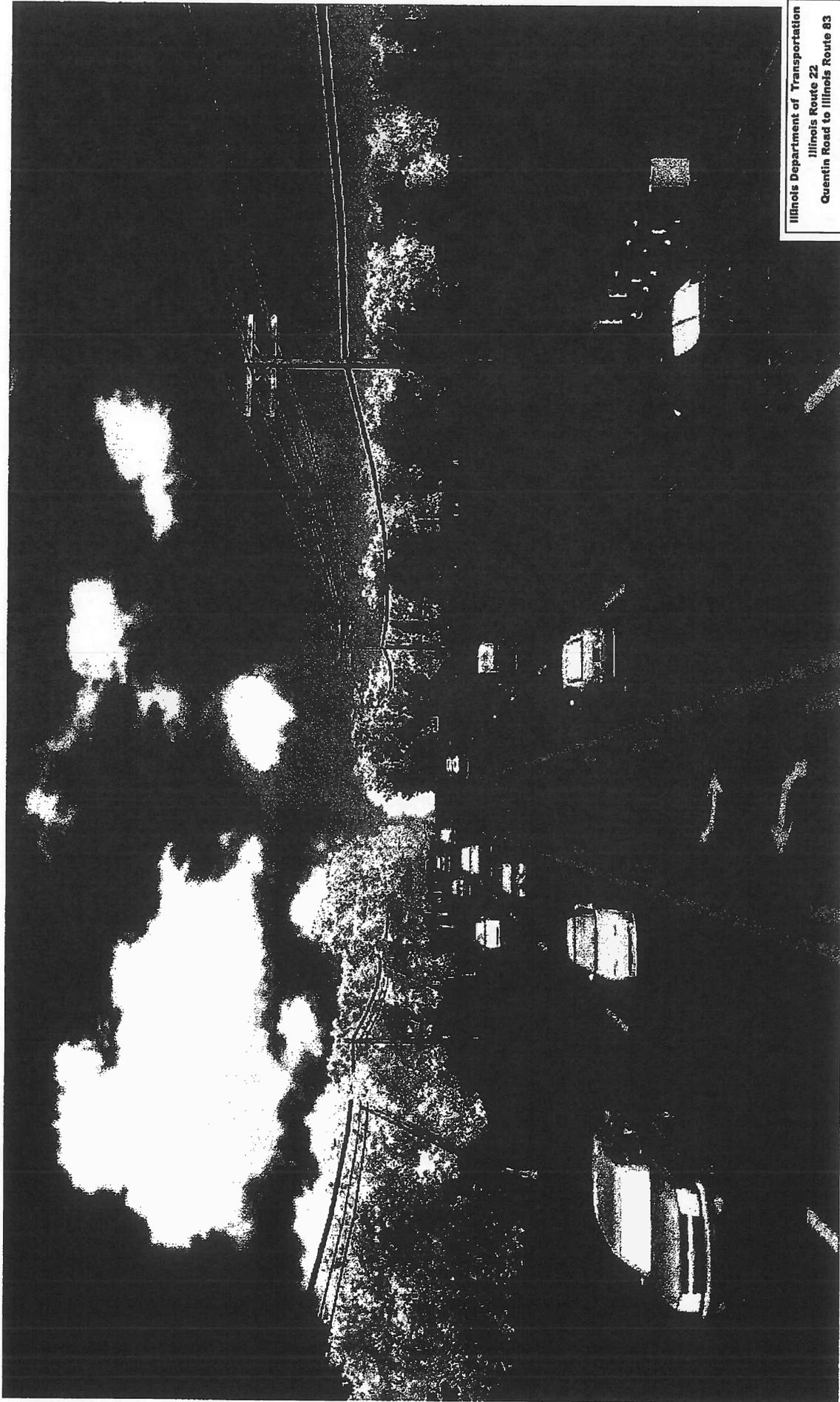
Notes:
 T/E = Temporary Construction Easement
 P/E = Permanent Easement
 SCE = Scenic Corridor Easement

⁽¹⁾ "Fill" indicates that the wall is retaining the roadway. "Cut" indicates that the wall is retaining the adjacent land.
⁽²⁾ Height Range is expressed as viewed from both the roadway side, and from the opposite side. All heights include a 3.5' extension for pedestrian, bicyclist, and/or vehicle safety.
⁽³⁾ (Number of Trees on Village's Protected Species List)(Total Trees). Replacement trees can be planted in scenic corridor easements in some cases if permitted by Village.
⁽⁴⁾ Permanent Easements in the Scenic Corridor Easements may be able to be Temporary Easements if the Village agrees to maintain the swale.

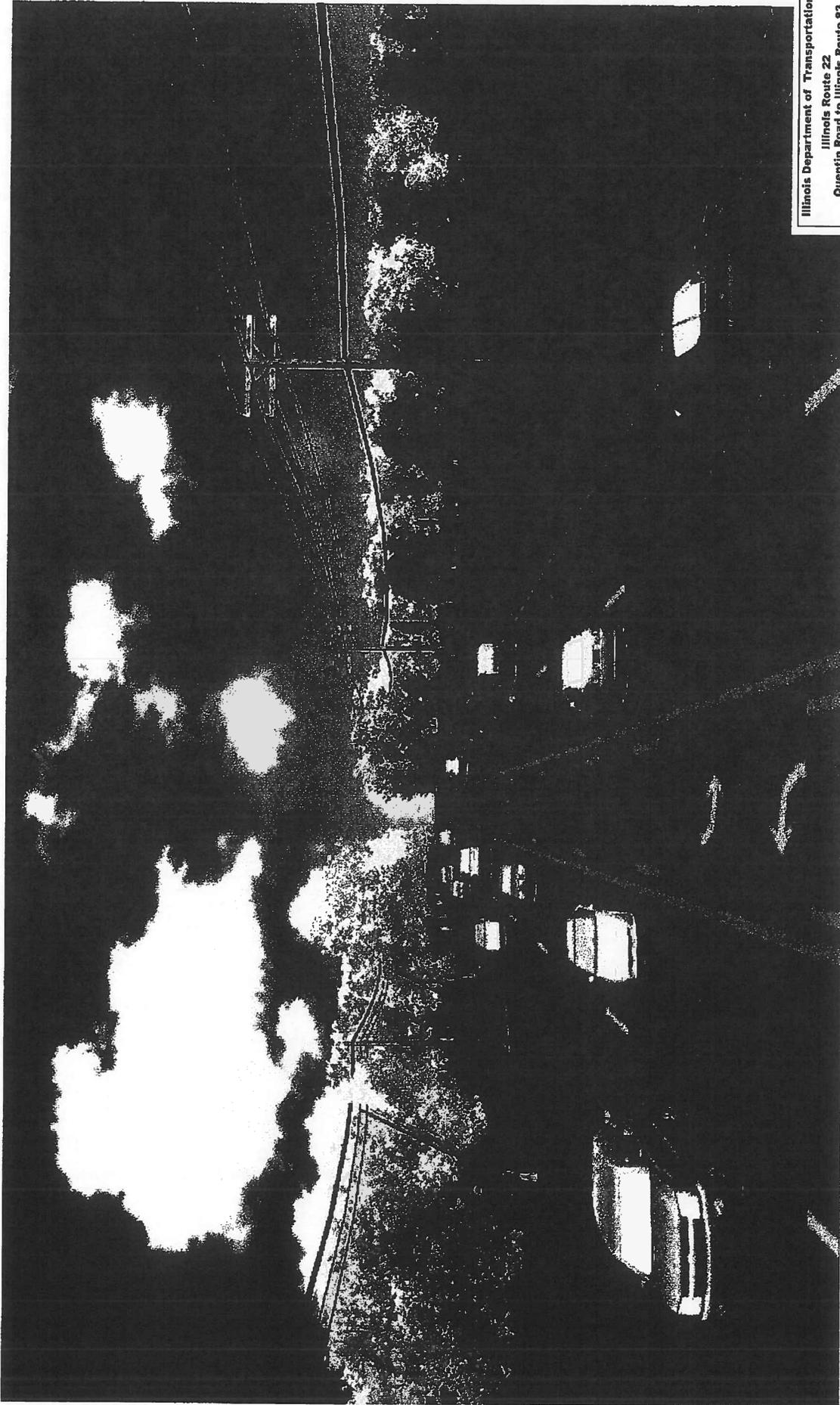
Attachment D – Colorized Typical Section Perspective Views



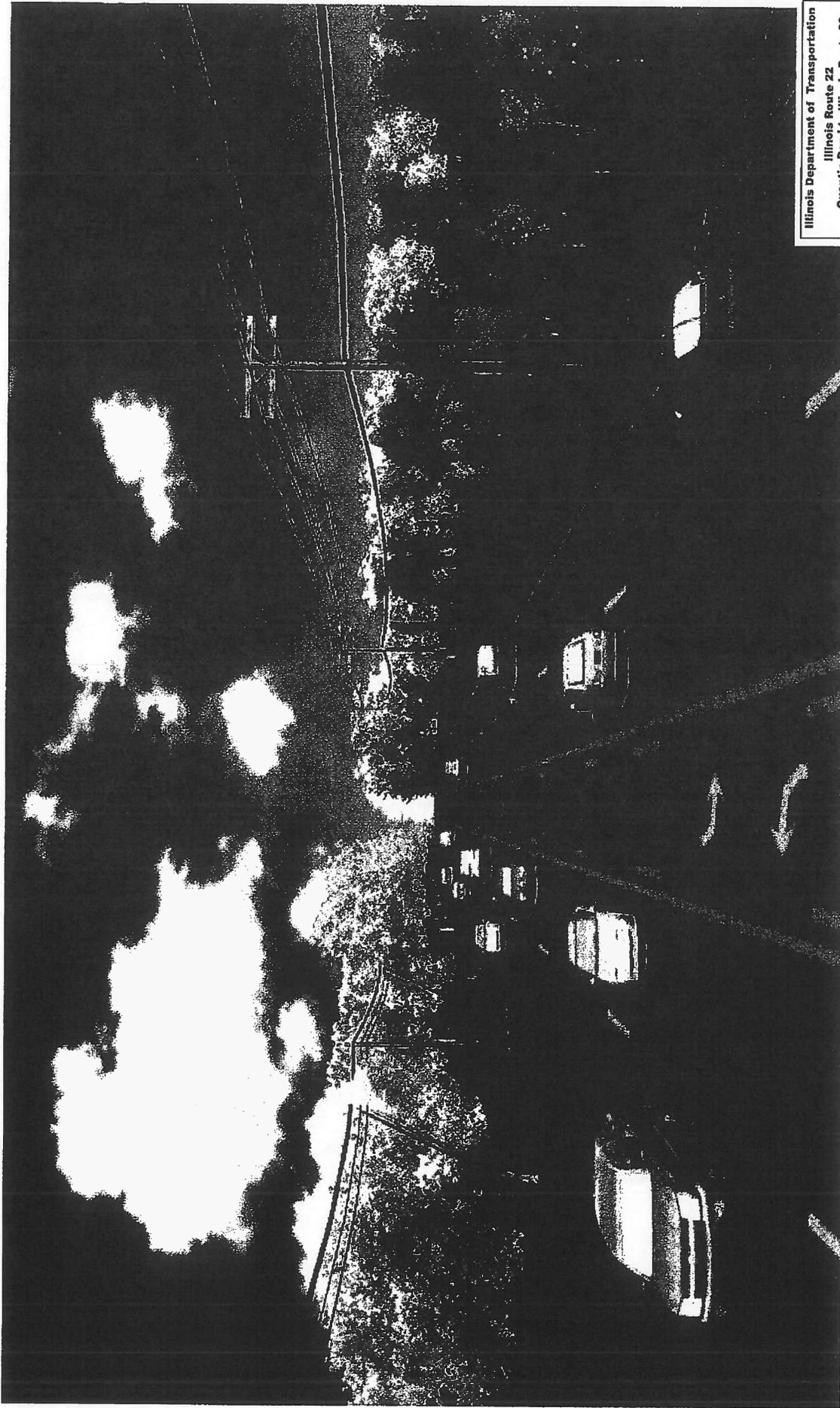
Illinois Department of Transportation
Illinois Route 22
Quentin Road to Illinois Route 83
EXISTING CONDITIONS
View 1
Date: 05/16/2010
Exhibit D-1



Illinois Department of Transportation
Illinois Route 22
Quentin Road to Illinois Route 83
**PROPOSED IMPROVEMENT
WITH RETAINING WALL**
View 1
Date: 06/16/2010
Exhibit D-2

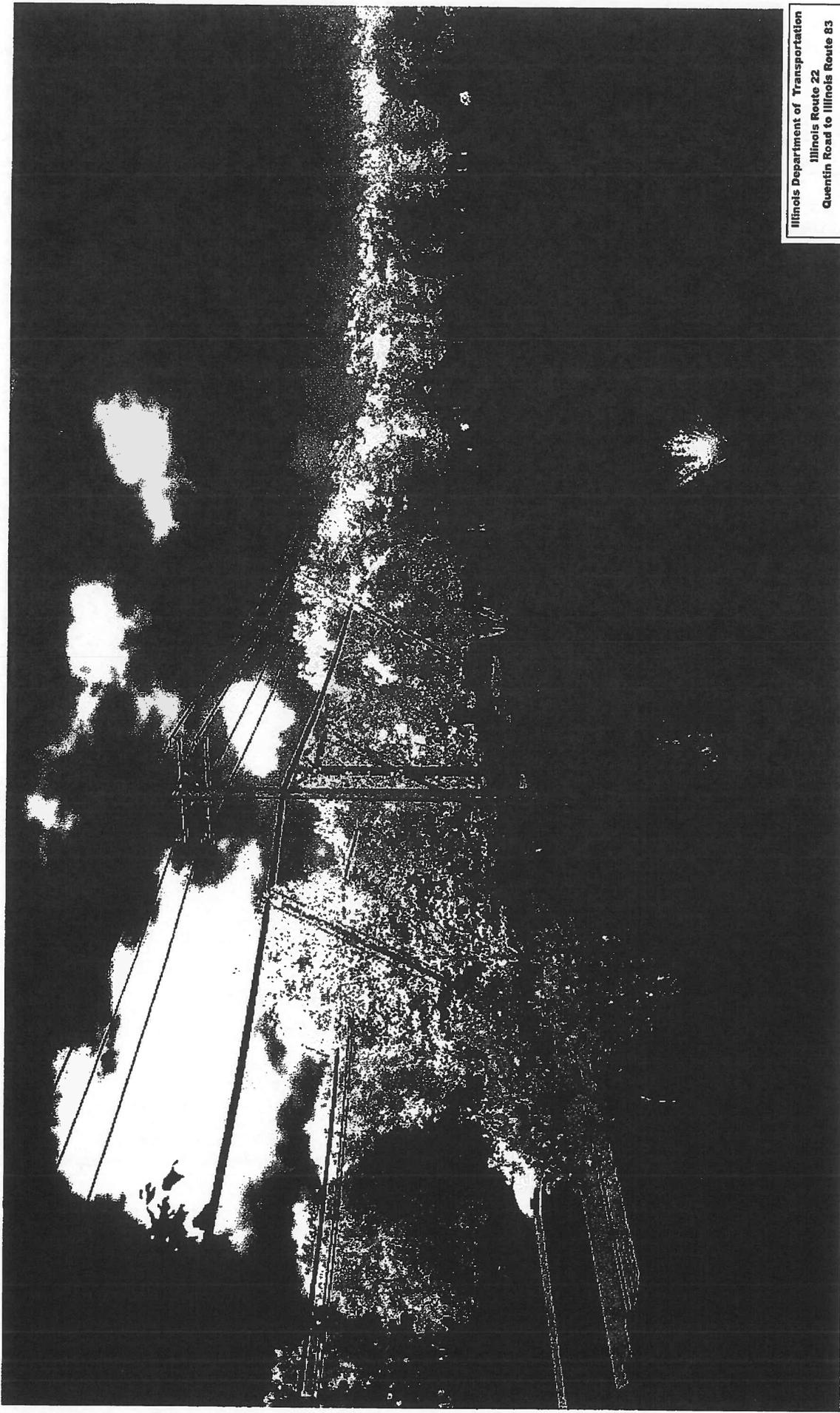


Illinois Department of Transportation
Illinois Route 22
Quentin Road to Illinois Route 83
**PROPOSED IMPROVEMENT
WITHOUT RETAINING WALL**
View 1
Date: 06/16/2010
Exhibit D-3



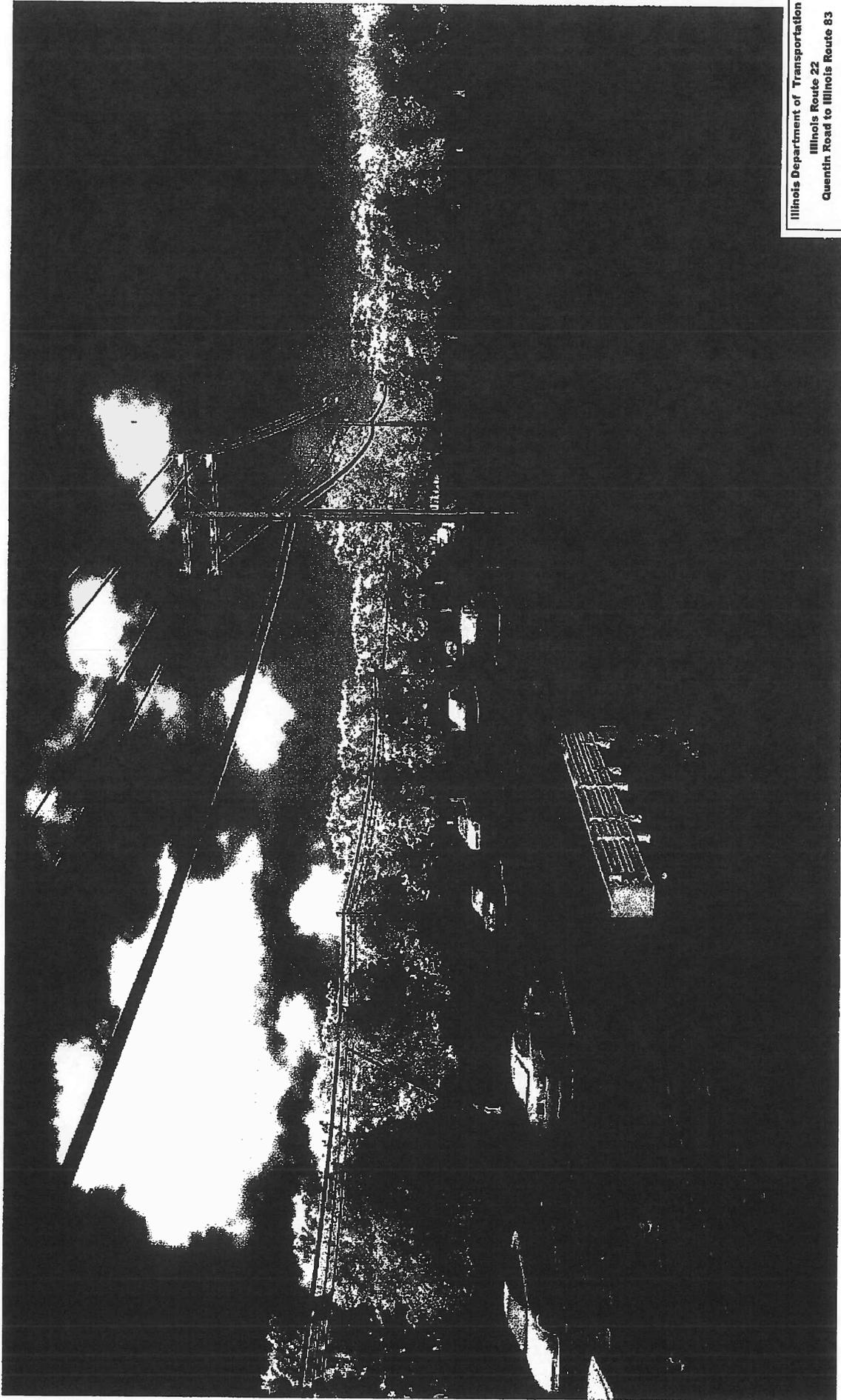
Illinois Department of Transportation
Illinois Route 22
Quentin Road to Illinois Route 83
**PROPOSED IMPROVEMENT
WITHOUT RETAINING WALL ***
View 1
Date: 06/16/2010
Exhibit D-4

* With tree replacement in
Scenic Corridor Easement

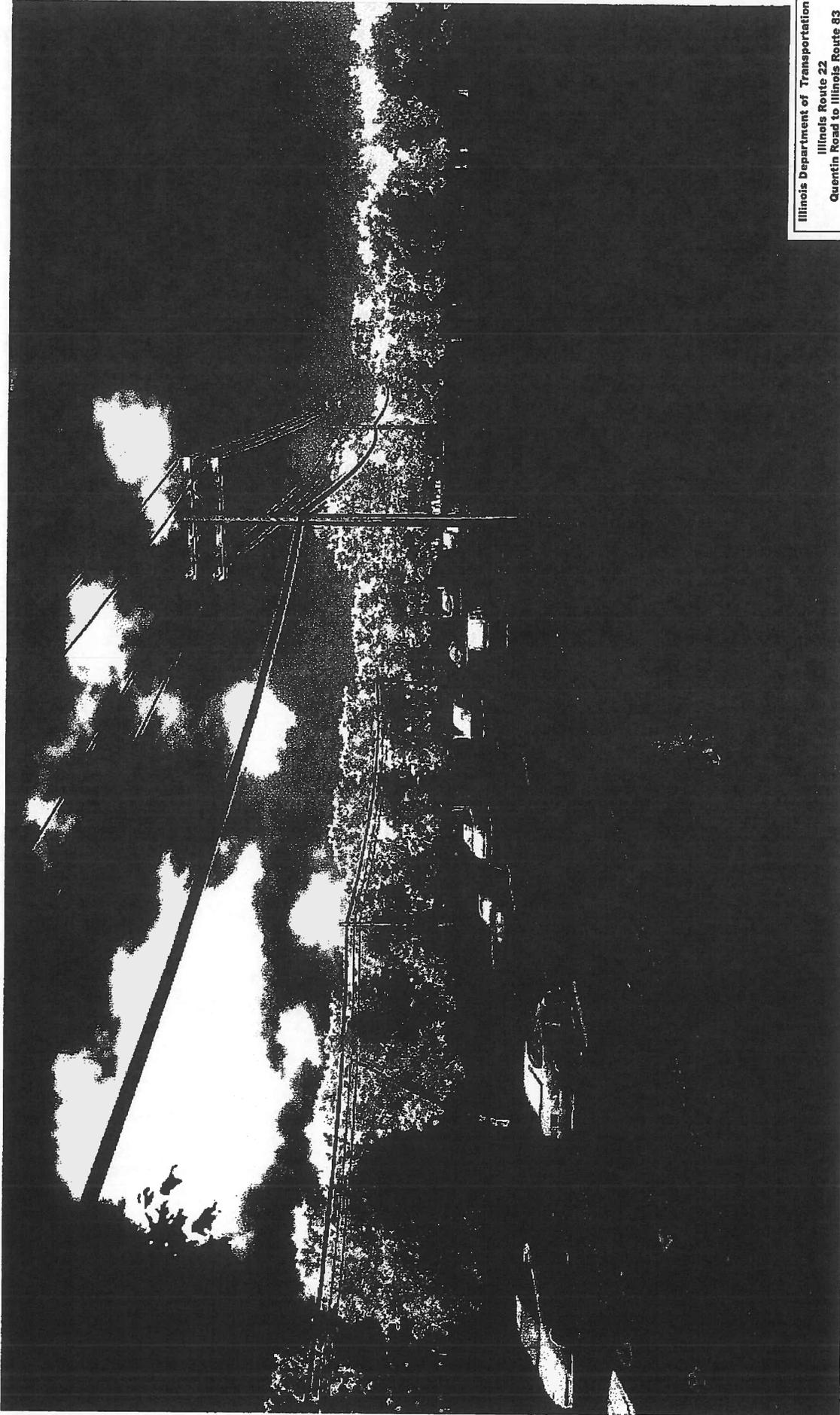


Illinois Department of Transportation
Illinois Route 22
Quentin Road to Illinois Route 83
EXISTING CONDITIONS
View 2

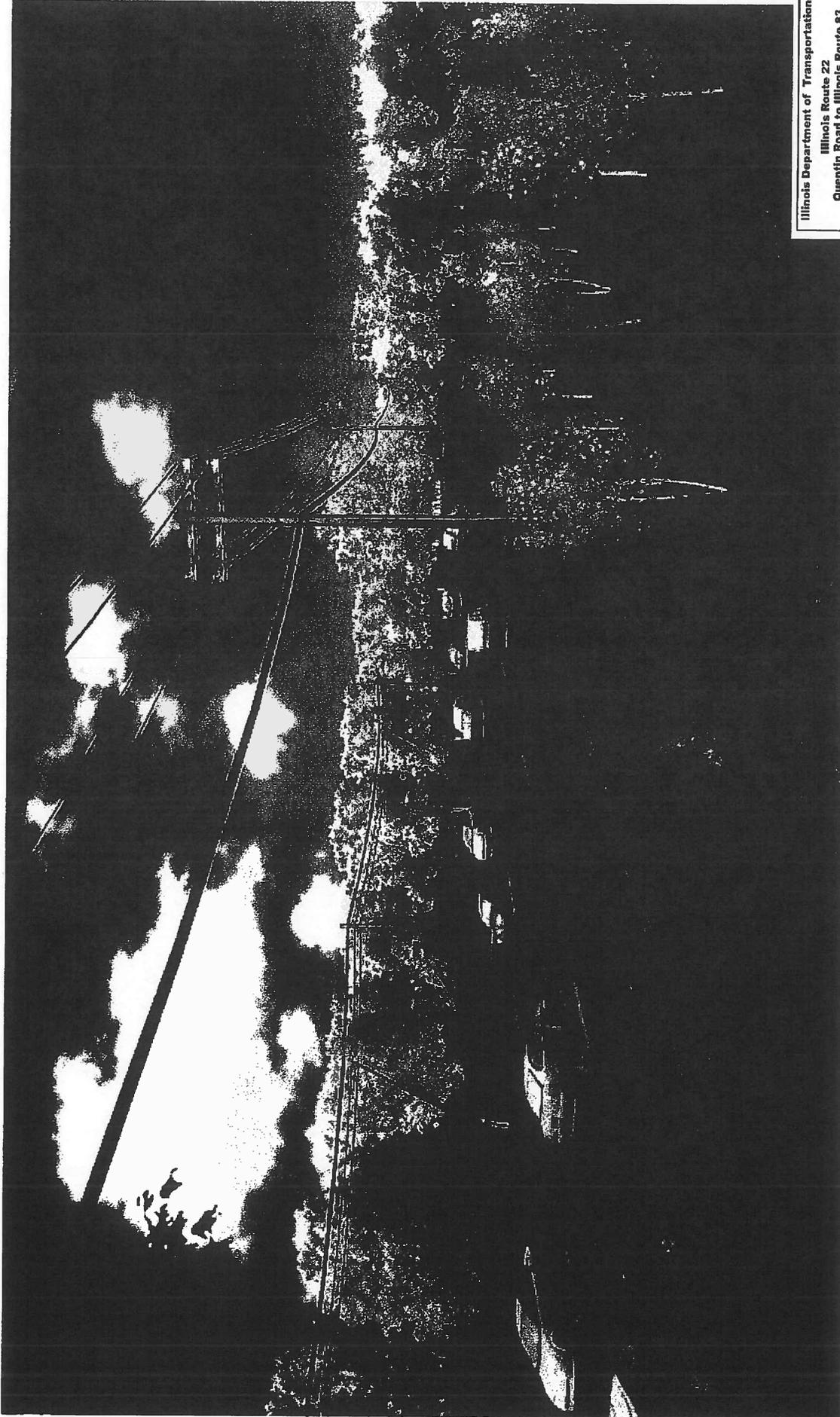
Date: 06/16/2010
Exhibit D-5



Illinois Department of Transportation
Illinois Route 22
Quentin Road to Illinois Route 83
**PROPOSED IMPROVEMENT
WITH RETAINING WALL**
View 2
Date: 06/16/2010
Exhibit D-6

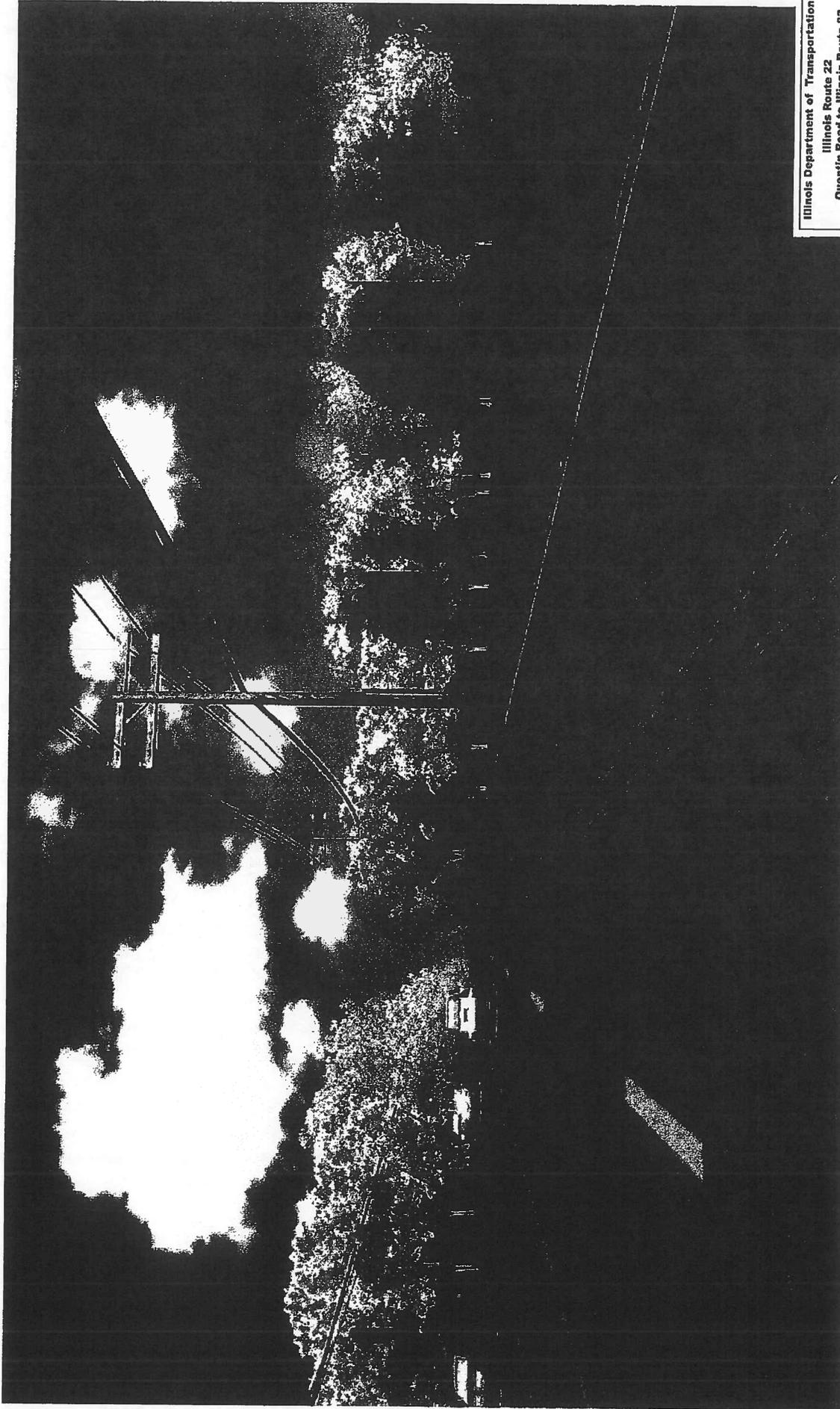


Illinois Department of Transportation
Illinois Route 22
Quentin Road to Illinois Route 83
**PROPOSED IMPROVEMENT
WITHOUT RETAINING WALL**
View 2
Date: 06/16/2010
Exhibit D-7



Illinois Department of Transportation
Illinois Route 22
Quentin Road to Illinois Route 83
**PROPOSED IMPROVEMENT
WITHOUT RETAINING WALL ***
View 2
Date: 06/16/2010
Exhibit D-8

* With tree replacement in
Scenic Corridor Easement



Illinois Department of Transportation
Illinois Route 22
Quentin Road to Illinois Route 83
**PROPOSED IMPROVEMENT
WITH RETAINING WALL**
View 3
Date: 06/16/2010
Exhibit D-9